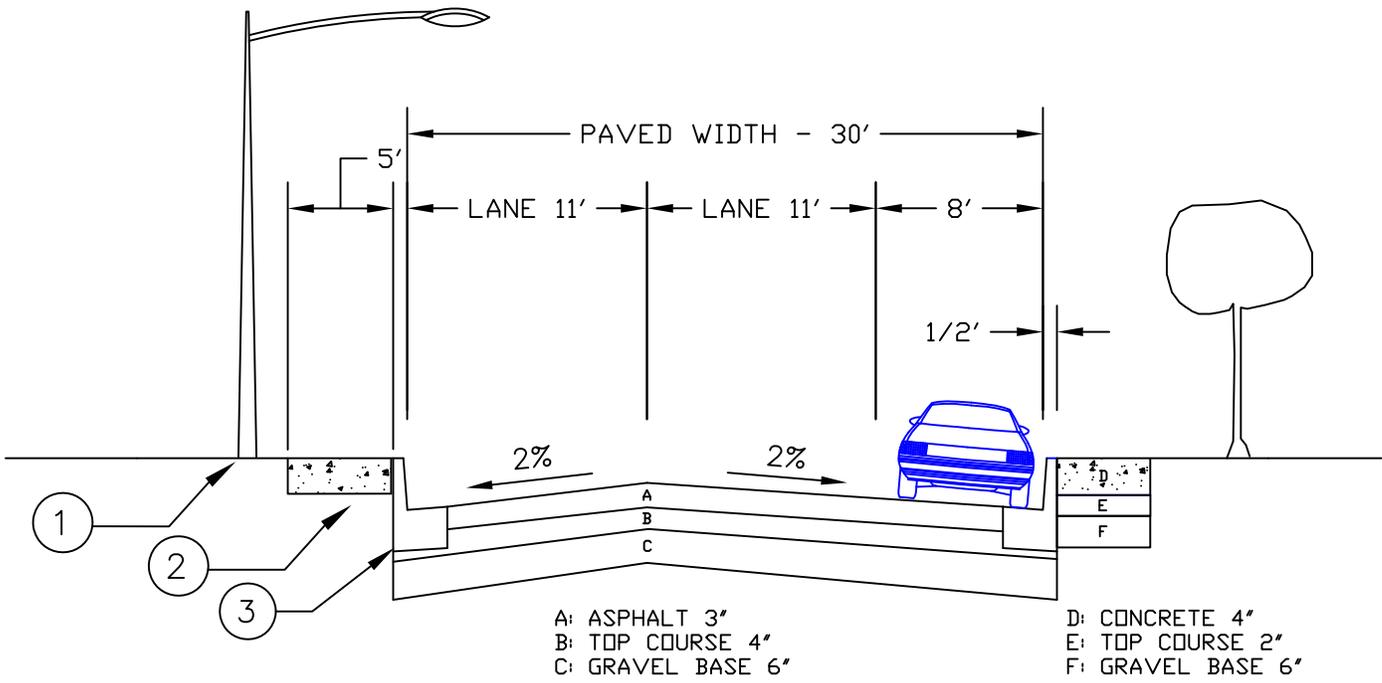


Design Standard	Neighborhood/ Commercial Collector	Residential Collector	Residential Access	Commercial Access
Figure	2-02	2-03	2-04	2-05
ROW Width	50' two lanes 60' three lanes	50'	40'; or 45' if sidewalks required on both sides	As required
Pavement Width	30' two lanes 42' three lanes	30'	28'	24'
Recommended ADT ¹	> 4000	400 - 4000	< 400	< 1000
Sidewalk	Both sides	Both sides	One side; both sides if required by City Engineer	As required by City Engineer
Lane width	12'	11'	10'	12'
Paved shoulder	3'	none	none	none
Connectivity	Yes	Yes	No	No
Maximum grade	12%	12%	12%	12%
Curb radii	35'	35'	25'	35'
On-street Parking	None unless 8' bulb-outs provided	8' one side	8' one side	None unless 8' bulb-outs provided

Figure 2-01. Local Access Streets

¹ Volumes based on 9.55 Average Daily Trips (ADT) per residential unit.

50' ROW



- ① PLANTING, LANDSCAPING AND STREET LIGHTING STRIP, BOTH SIDES
- ② CEMENT CONCRETE SIDEWALK, 5' - BOTH SIDES
- ③ CEMENT CONCRETE CURB & GUTTER (SEE WSDOT STD. PLANS)

TYPICAL ROADWAY SECTION
RESIDENTIAL COLLECTOR
 N.T.S.

DRAWINGS\542-ROADS\ROAD-PAVING\Res_Collector-smal-v01.dwg



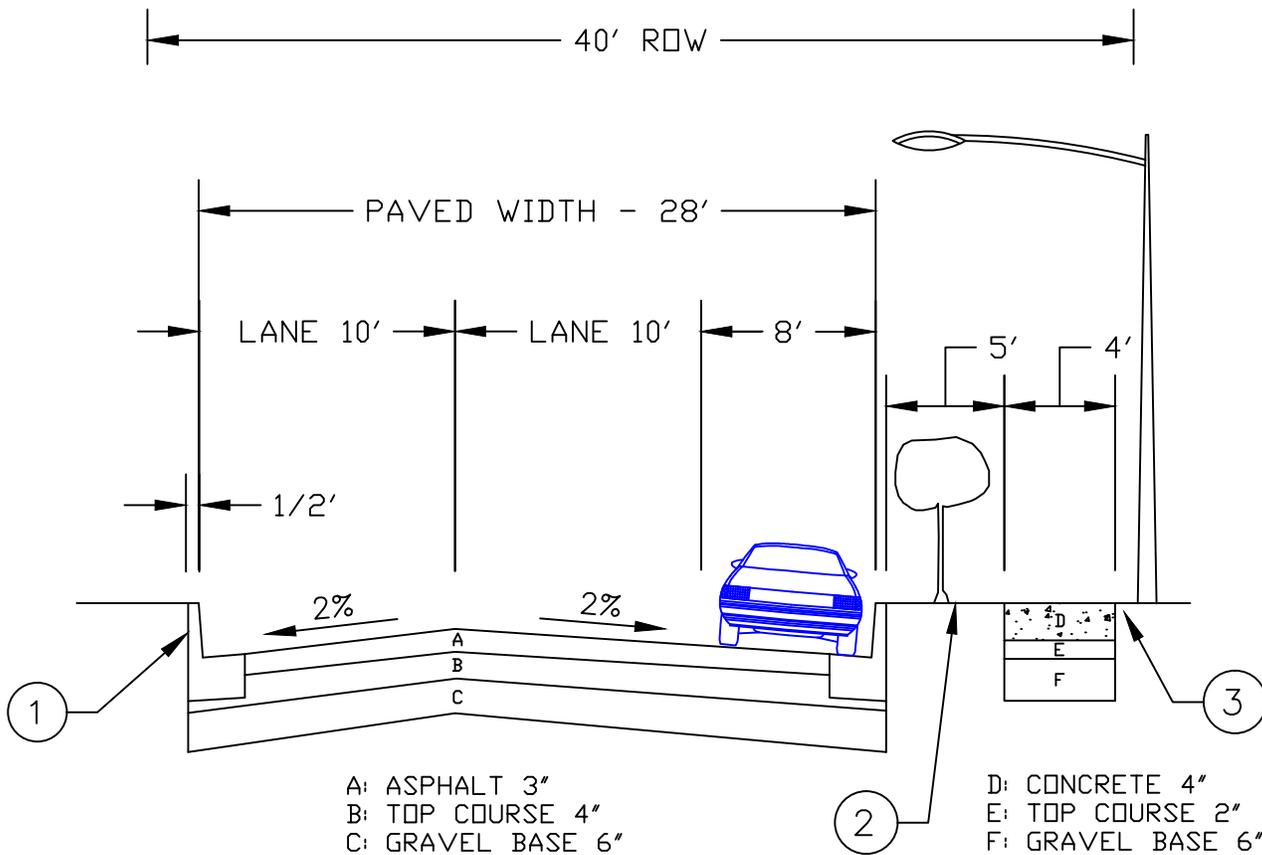
CITY OF POULSBO
 DEPARTMENT OF PUBLIC WORKS

TYPICAL ROADWAY SECTION
 RESIDENTIAL COLLECTOR

REVISED BY: RDF
 DATE: 10/13/08

APPROVED BY: _____
 DATE: _____

Fig. 2-03



A: ASPHALT 3"
 B: TOP COURSE 4"
 C: GRAVEL BASE 6"

D: CONCRETE 4"
 E: TOP COURSE 2"
 F: GRAVEL BASE 6"

- ① CEMENT CONCRETE CURB & GUTTER (SEE WSDOT STD. PLANS)
- ② PLANTING, LANDSCAPING AND STREET LIGHTING STRIP, ONE SIDE
- ③ CEMENT CONCRETE SIDEWALK, 4' - ONE SIDE. SIDEWALK SHALL BE 5' WIDE IF NOT SEPERATED FROM TRAVEL LANES BY PARKING STRIP, LANDSCAPING, BIOSWALE, OR RAINGARDENS.

TYPICAL ROADWAY SECTION
 RESIDENTIAL ACCESS
 DETACHED SIDEWALK, ONE SIDE
 N.T.S.



CITY OF POULSBO
 DEPARTMENT OF PUBLIC WORKS

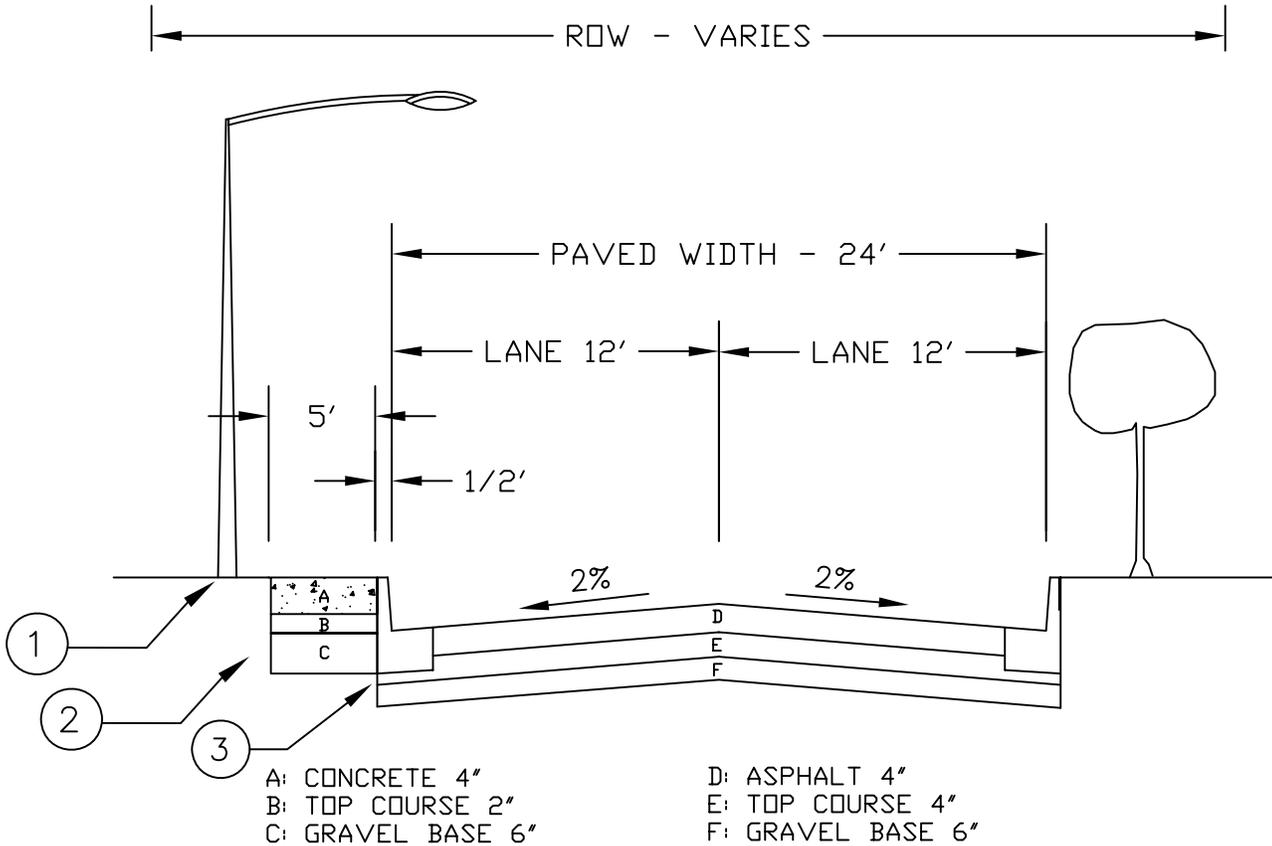
TYPICAL ROADWAY SECTION
 RESIDENTIAL ACCESS
 SIDEWALK ONE-SIDE

REVISED BY: RDF
 DATE: 10/13/08

APPROVED BY: _____
 DATE: June 1, 2009

Fig. 2-04
 REF: P-yyy

\Drawings\542Roads\Roads-Paving\Residential_L_Access--Small--SW1side.dwg
 REVISED: 3/11/2005



- ① PLANTING, LANDSCAPING AND STREET LIGHTING STRIP, BOTH SIDES
- ② CEMENT CONCRETE SIDEWALK, 5' - AS REQUIRED
- ③ CEMENT CONCRETE CURB & GUTTER (SEE WSDOT STD. PLANS)

TYPICAL ROADWAY SECTION
COMMERCIAL ACCESS
SIDEWALK ONE SIDE

N.T.S.



CITY OF POULSBO
 DEPARTMENT OF PUBLIC WORKS

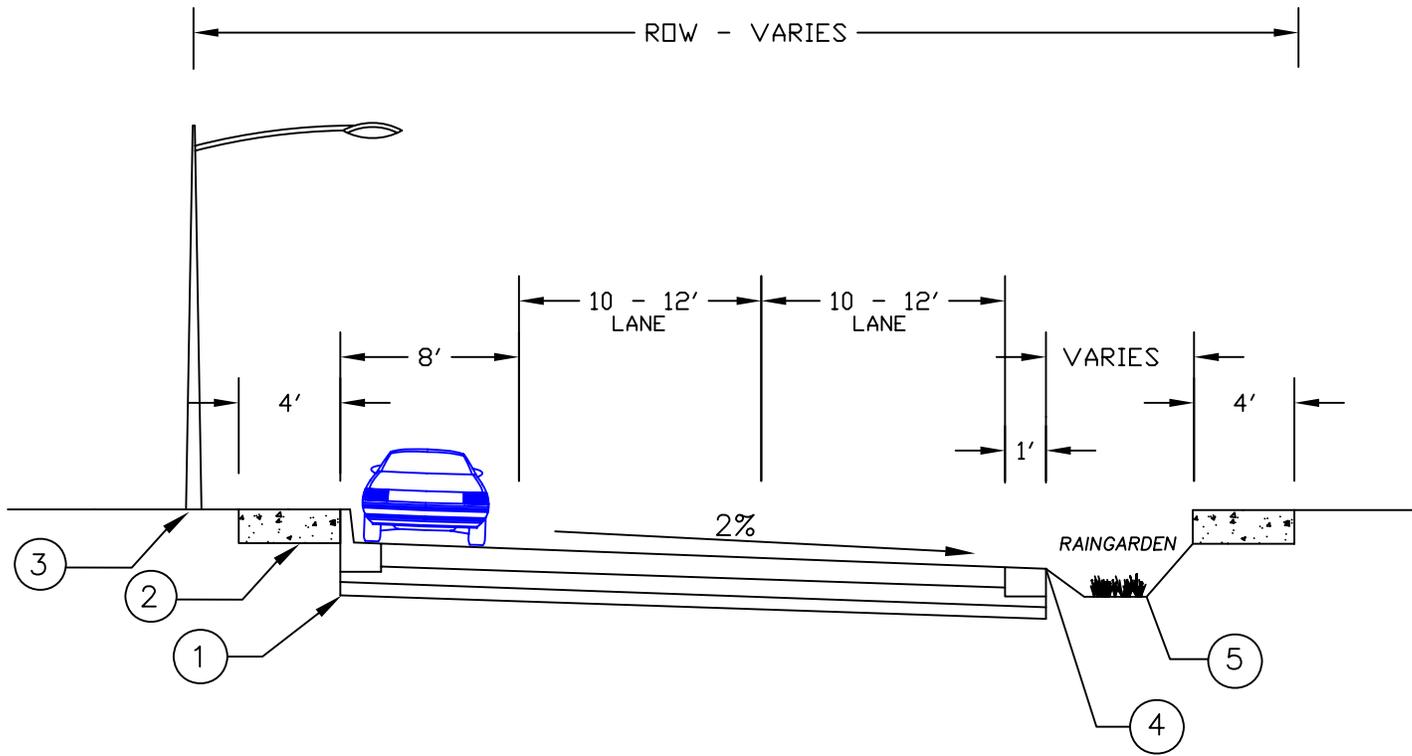
TYPICAL ROADWAY SECTION
 COMMERCIAL ACCESS
 SIDEWALK ONE SIDE

REVISED BY: RDF
 DATE: 10/13/08

APPROVED BY: _____
 DATE: June 1, 2009

Fig. 2-05
 REF: P-yyy

REVISION: 3/11/2005 \Drawings\542Roads\Roads-Paving\Residential_Access-Small-SW1side.dwg



- ① CEMENT CONCRETE CURB & GUTTER (SEE WSDOT STD. PLANS)
- ② CEMENT CONCRETE SIDEWALK, 4'
- ③ PLANTING, LANDSCAPING AND STREET LIGHTING STRIP, BOTH SIDES
- ④ FLUSH CEMENT CONCRETE CURB
- ⑤ RAINGARDEN IN ACCORDANCE WITH STORMWATER DESIGN STANDARDS

TYPICAL ROADWAY SECTION
LOW IMPACT DEVELOPMENT
N.T.S.

NOTE: LID TECHNIQUES MAY BE USED ON ANY LOCAL ACCESS STREET DESCRIBED IN FIGURE 2-01 PER THE DISCRETION OF THE CITY ENGINEER



CITY OF POULSBO
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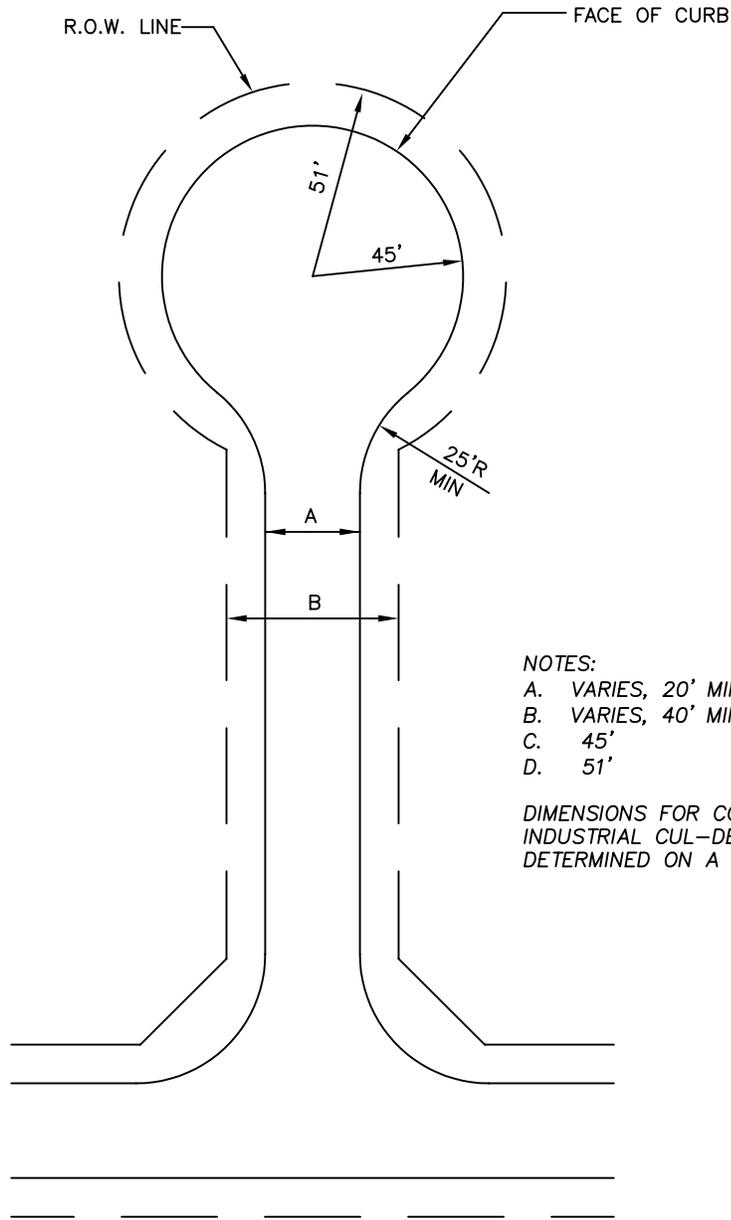
TYPICAL ROADWAY SECTION
LOW IMPACT DEVELOPMENT

REVISED BY: RDF
DATE: 10/13/08

APPROVED BY: _____
DATE: June 1, 2009

Fig. 2-08
REF: P-yyy

REVISED:5/2/2005/2005-JAL file: \2-10.dwg



- NOTES:
 A. VARIES, 20' MINIMUM
 B. VARIES, 40' MINIMUM, SEE FIGURE 2-01
 C. 45'
 D. 51'

DIMENSIONS FOR COMMERCIAL AND INDUSTRIAL CUL-DE-SACS WILL BE DETERMINED ON A CASE-BY-CASE BASIS.

THE MAXIMUM LENGTH OF A CUL-DE-SAC SHALL NOT EXCEED 500'.

TYPICAL CUL-DE-SAC

N.T.S.



CITY OF POULSBO
 DEPARTMENT OF PUBLIC WORKS

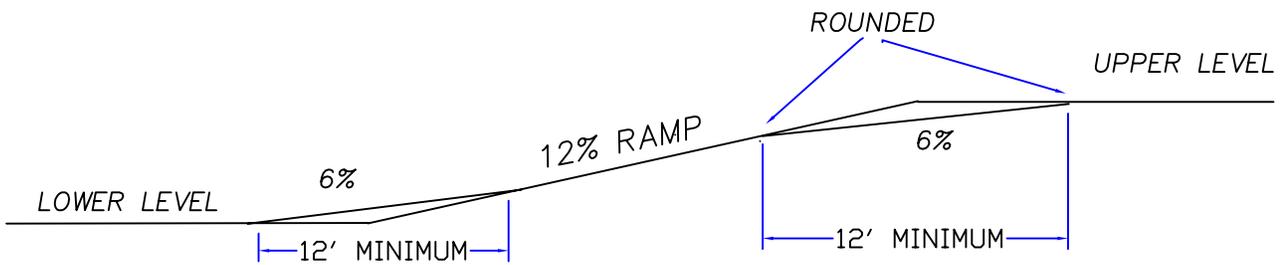
RESIDENTIAL CUL-DE-SAC

REVISED BY: JAL
 DATE: 5/2/2005

APPROVED BY: _____
 DATE: June 1, 2009

Fig. 2-10
 REF: P-307

REVISED: 3/28/2005--JAL file: 2-14.dwg



TRANSITION REQUIRED WHEN ALGEBRAIC DIFFERENCE OF GRADES EXCEEDS 6%

NOTES:
This type of transition must be constructed for all ramps between floors, parking areas, streets, etc., to allow adequate clearances for long overhang vehicles.



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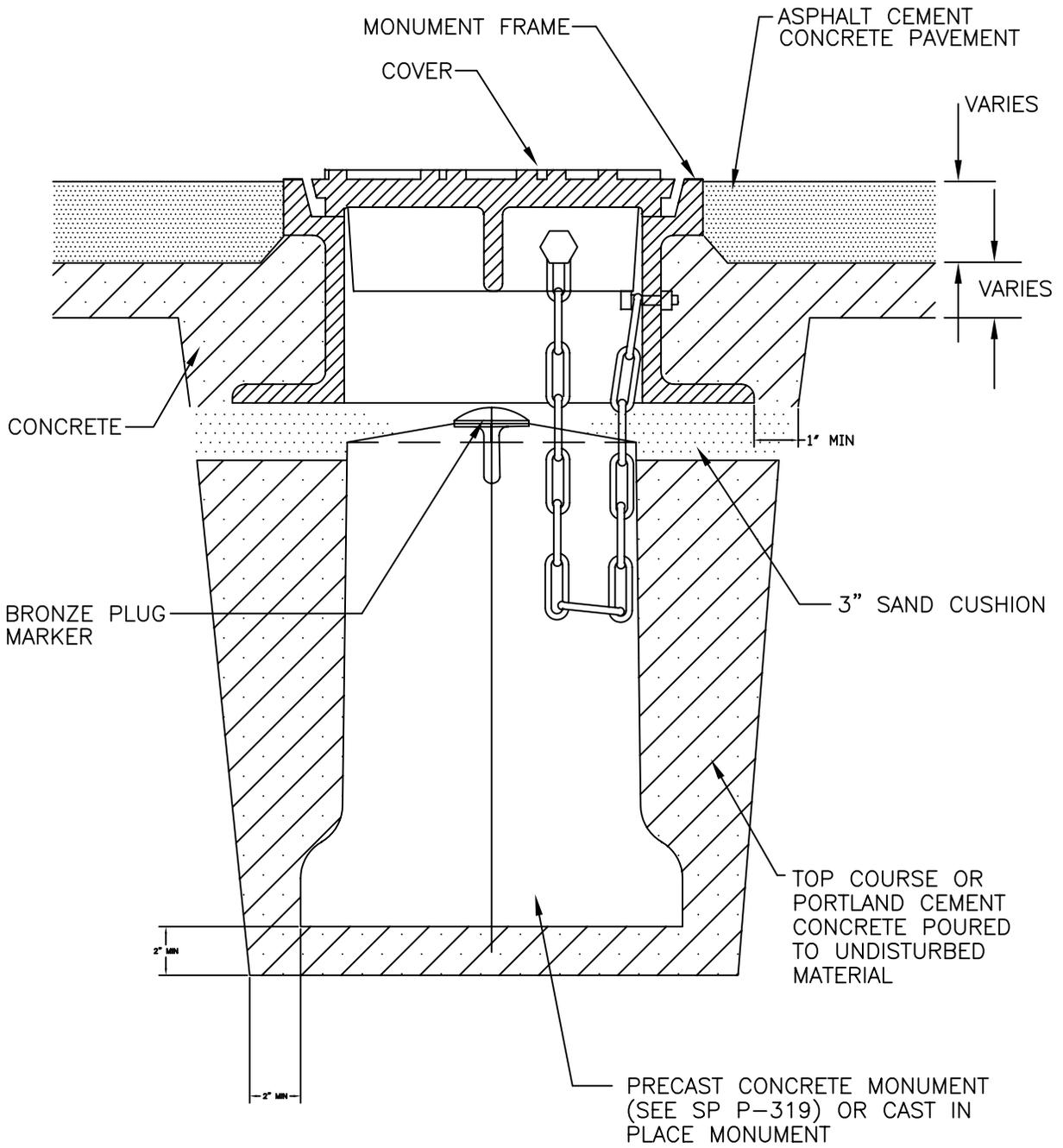
GRADE TRANSITION

REVISED BY: JAL
DATE: 3/28/2005

APPROVED BY: _____
DATE: June 1, 2009

Fig. 2-14
Ref: Poulsbo H-10

REVISED: 3/28/2005-JAL file: \2-16.dwg



CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

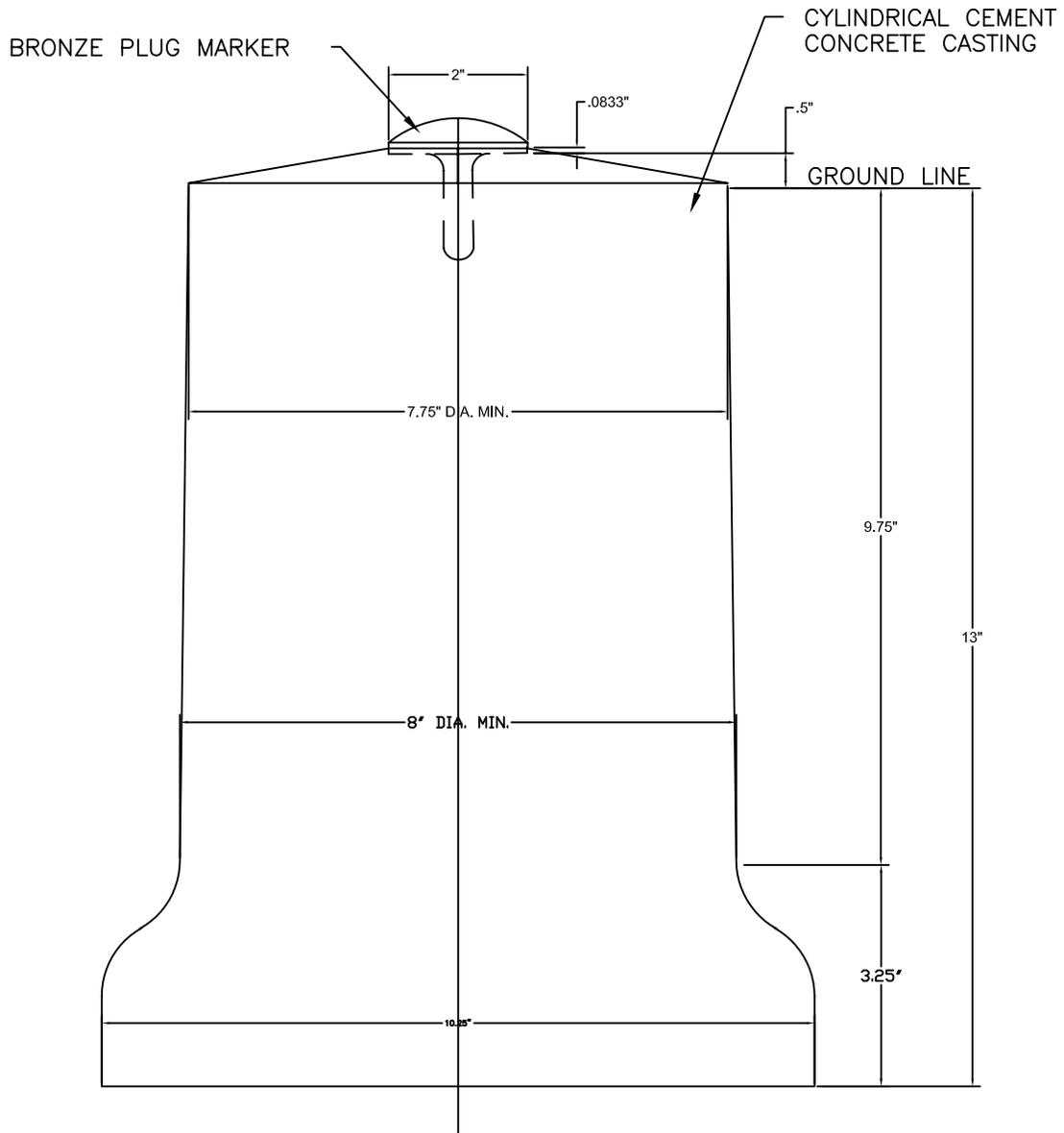
MONUMENT INSTALLATION

REVISED BY: JAL
DATE: 3/28/2005

APPROVED BY: _____
DATE: June 1, 2009

Fig. 2-16
REF: P-318

REVISED: 3/28/2005-JAL file: \2-17.dwg



PRECAST CONCRETE
MONUMENT
 N.T.S.
 CITY DRAWING P-319



CITY OF POULSBO
 DEPARTMENT OF PUBLIC WORKS

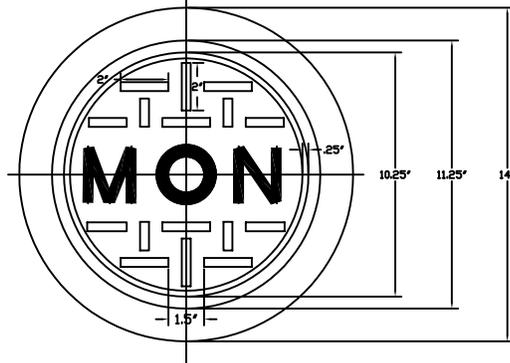
PRECAST CONCRETE MONUMENT

REVISED BY: JAL
 DATE: 3/28/2005

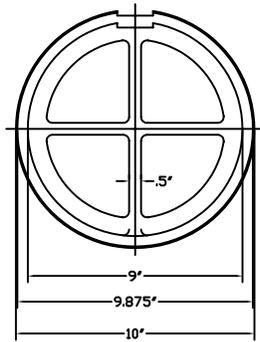
APPROVED BY: _____
 DATE: June 1, 2009

Fig. 2-17
 REF: P-319

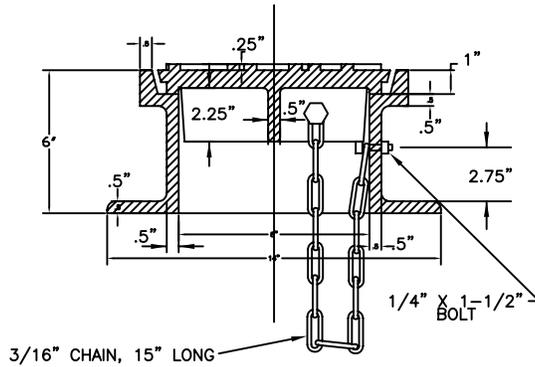
REVISED: 3/28/2005-JAL file: \2-18.dwg



TOP VIEW-FRAME & COVER



BOTTOM VIEW OF COVER



SECTION-FRAME & COVER

CONFORMS TO APWA PLAN # 19

MONUMENT
 FRAME & COVER
 N.T.S.
 CITY DRAWING B-320



CITY OF POULSBO
 DEPARTMENT OF PUBLIC WORKS

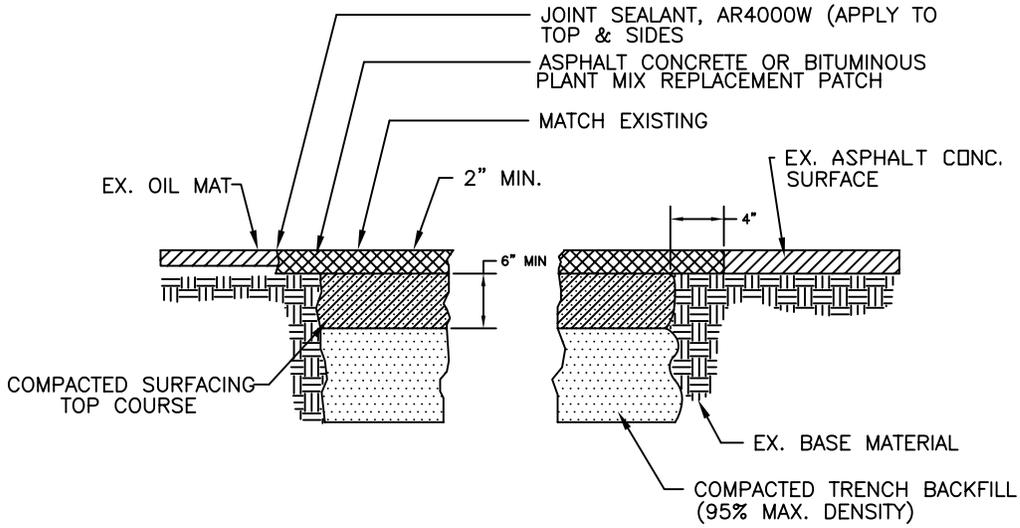
MONUMENT FRAME AND COVER

REVISED BY: JAL
 DATE: 3/28/2005

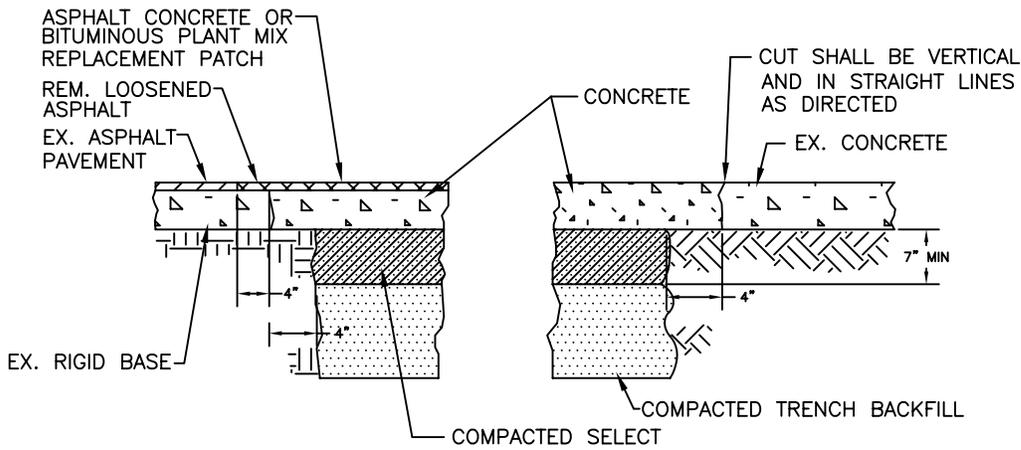
APPROVED BY: _____
 DATE: June 1, 2009

Fig. 2-18
 REF: P-320

REVISED: 3/28/2005-JAL file: \2-30.dwg



TYPICAL ASPHALT CONCRETE
PAVEMENT PATCH



TYPICAL CEMENT CONCRETE
PAVEMENT RESTORATION

TYPICAL PAVEMENT RESTORATION
N.T.S.



CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

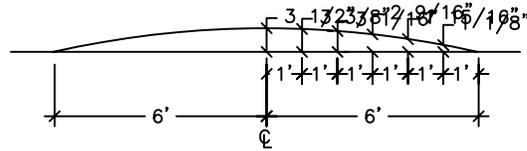
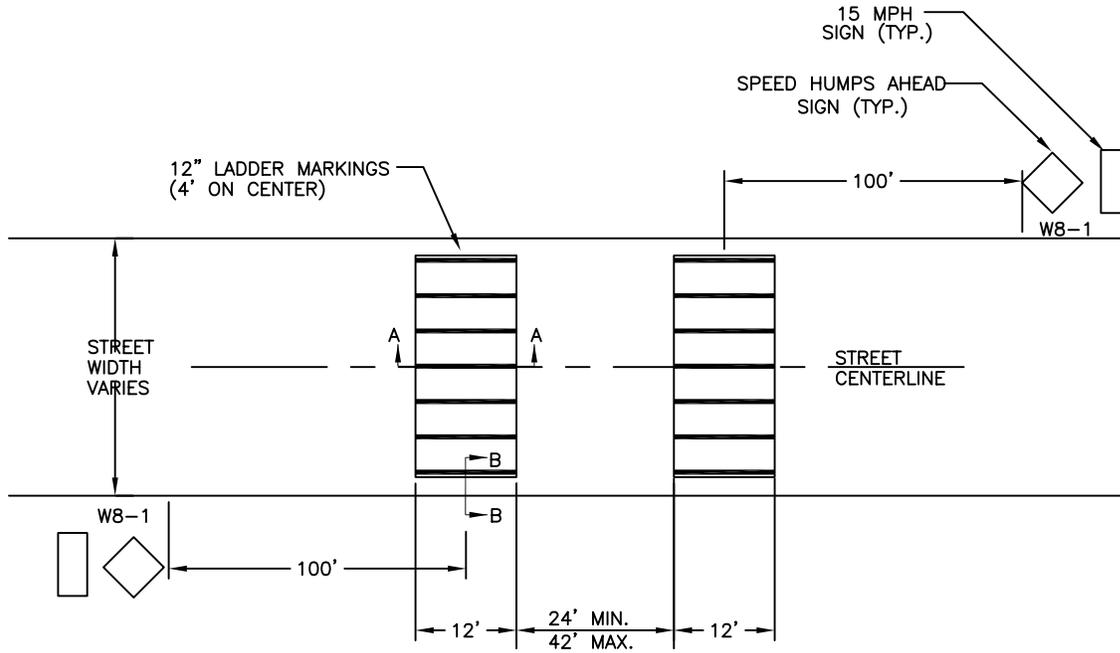
TYPICAL PAVEMENT RESTORATION

REVISED BY: JAL
DATE: 3/28/2005

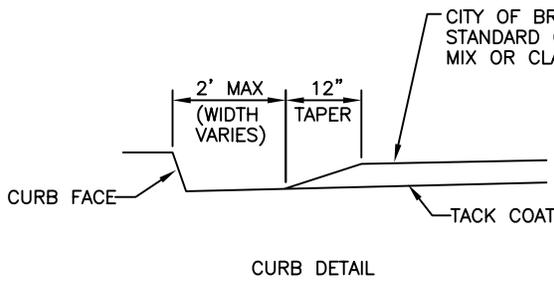
APPROVED BY: _____
DATE: June 1, 2009

Fig. 2-30
REF: P-333

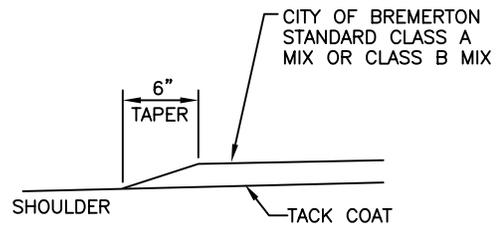
REVISED: 3/28/2005-JAL file: \2-31.dwg



PARABOLIC CROWN
SECTION A-A



CURB DETAIL



SHOULDER DETAIL FOR STREETS WITHOUT CURBS

SECTION B-B
SPEED HUMP DETAIL
N.T.S.



CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

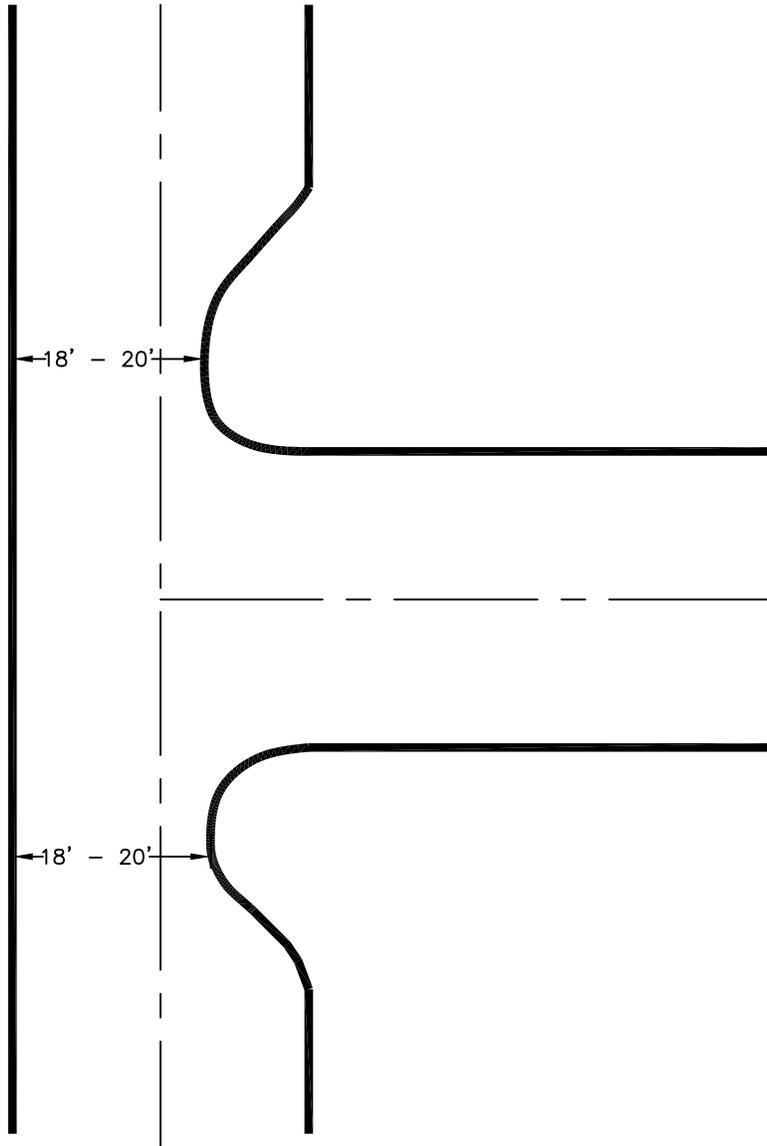
TRAFFIC CALMING
SPEED HUMP

REVISED BY: JAL
DATE: 3/28/2005

APPROVED BY: _____
DATE: _____

Fig. 2-31
REF: P-371

REVISED: 3/28/2005-JAL file: \2-32.dwg



CURB "BULB OUTS"
N.T.S.



CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

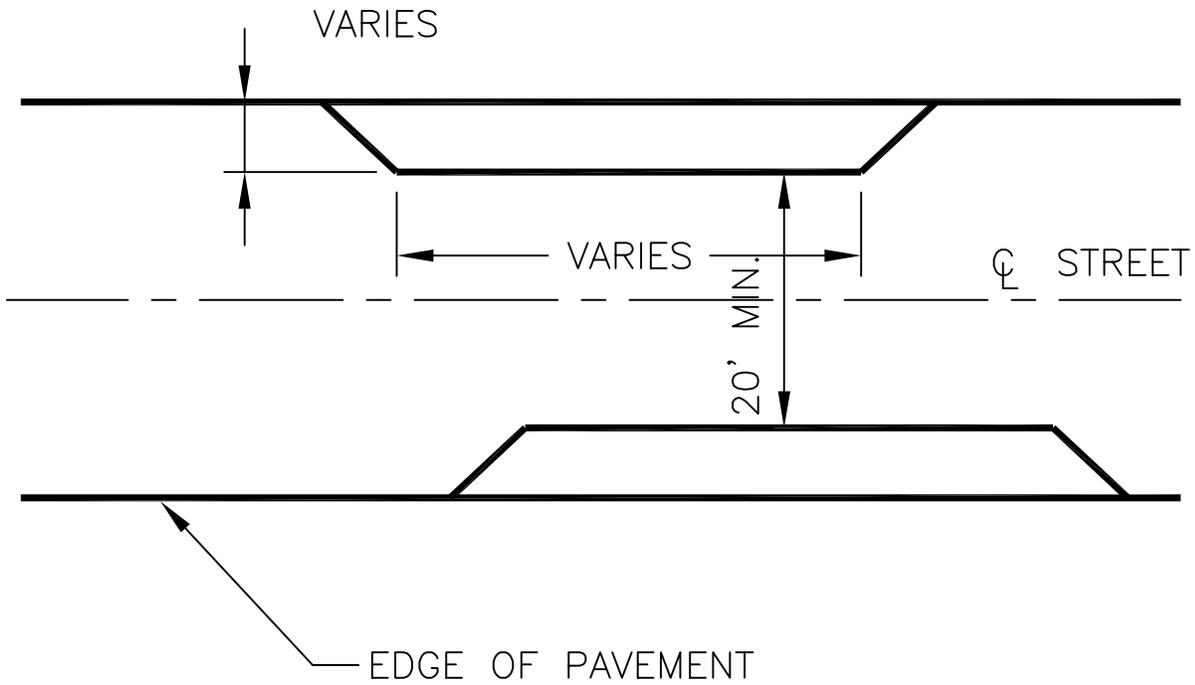
TRAFFIC CALMING
BULB-OUT

REVISED BY: JAL
DATE: 3/28/2005

APPROVED BY: _____
DATE: _____

Fig. 2-32
REF: P-372

REVISED: 3/28/2005-JAL file: \2-33.dwg



TWO-LANE SLOW POINT
 N.T.S.
 CITY DRAWING P-373



CITY OF POULSBO
 DEPARTMENT OF PUBLIC WORKS

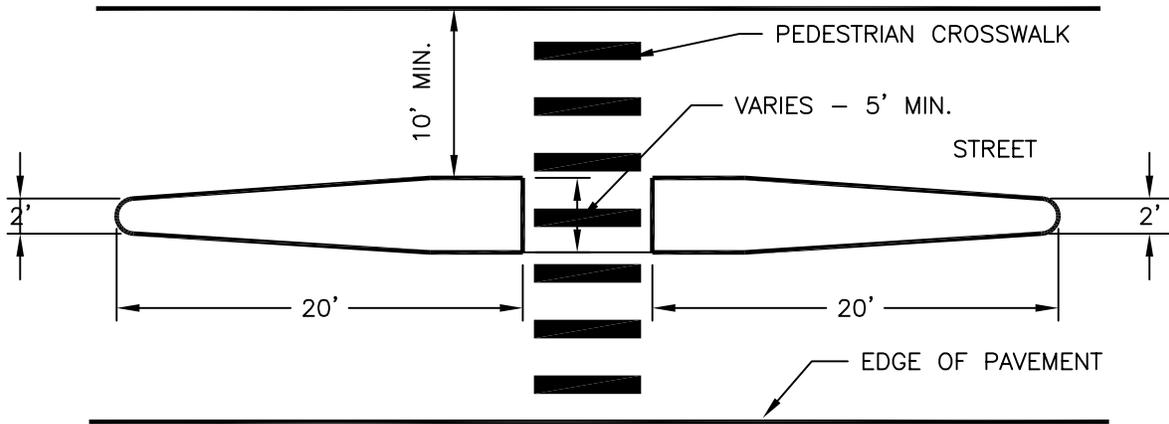
TRAFFIC CALMING
 TWO-LANE SLOW POINT

REVISED BY: JAL
 DATE: 3/28/2005

APPROVED BY: _____
 DATE: _____

Fig. 2-33
 REF: P-373

REVISED: 3/28/2005-JAL file: \2-34.dwg



MID-BLOCK MEDIAN
N.T.S.



CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

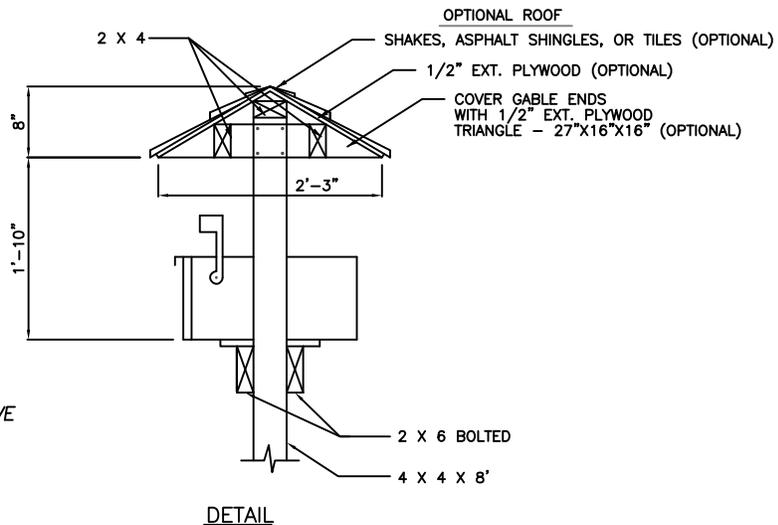
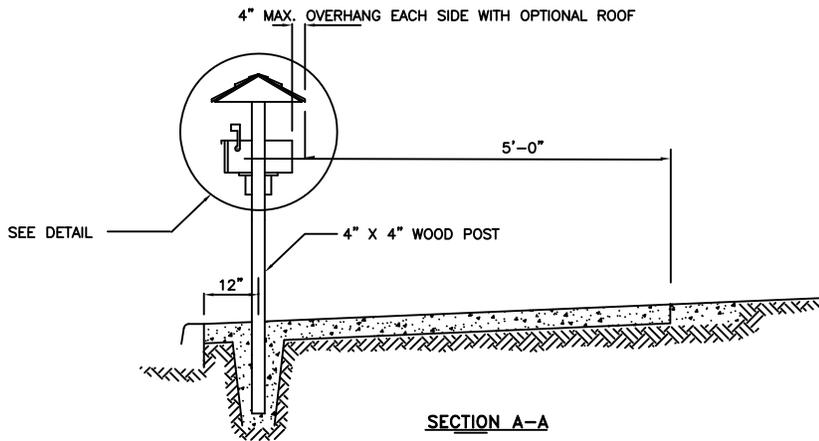
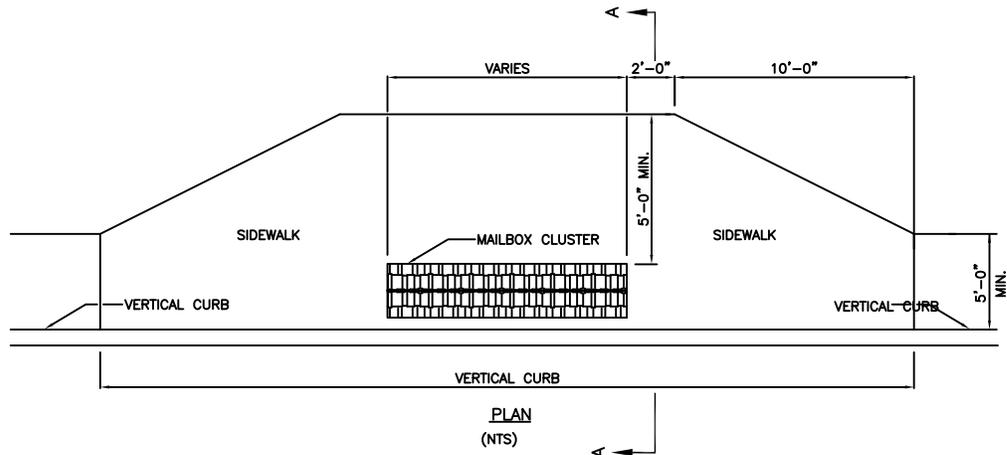
TRAFFIC CALMING
MID-BLOCK MEDIAN

REVISED BY: JAL
DATE: 3/28/2005

APPROVED BY: _____
DATE: _____

Fig. 2-34
REF: P-374

REVISED: 3/28/2005-JAL file: \XXXX.dwg



NOTES

- 1. BASE OF MAILBOX 38" - 42" ABOVE STREET FINISHED LEVEL.
- 2. ROOF STRUCTURE OPTIONAL



CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

MAILBOX LOCATION

REVISED BY: JAL
DATE: 3/28/2005

APPROVED BY: _____
DATE: June 1, 2009

Fig. 2-35
REF: KING COUNTY
M2012

NOTES:

(1) POST SPECIFICATIONS:

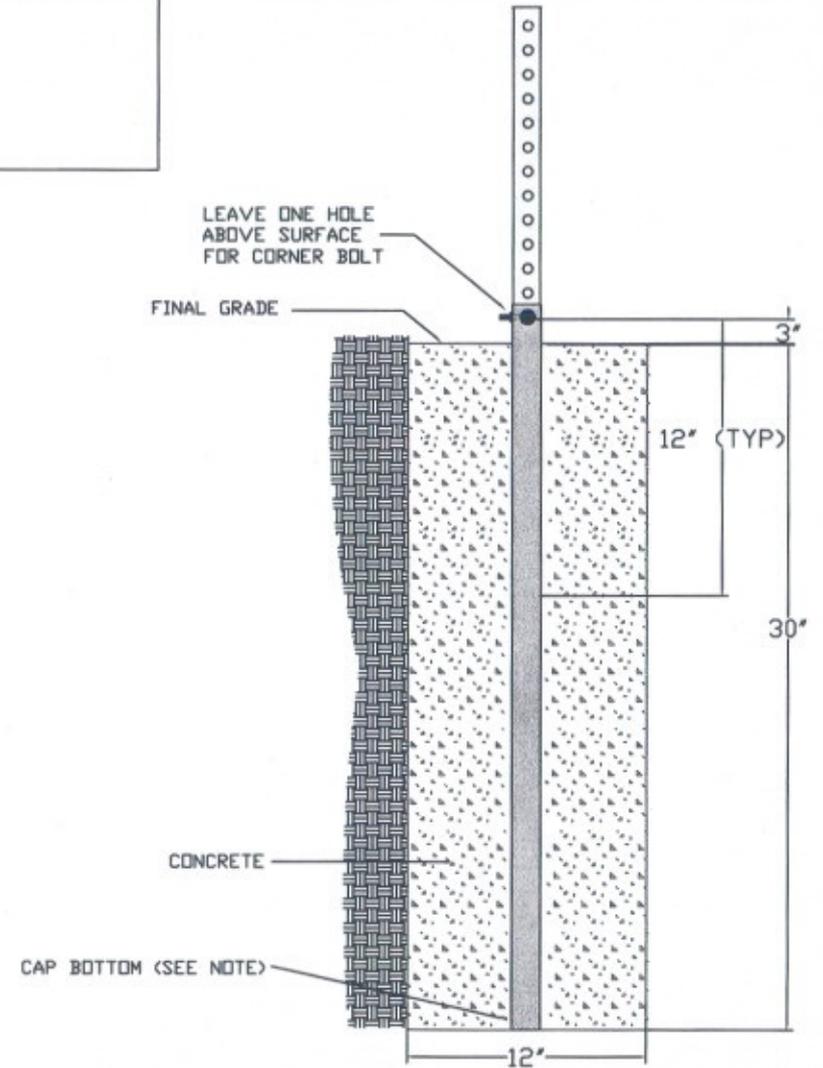
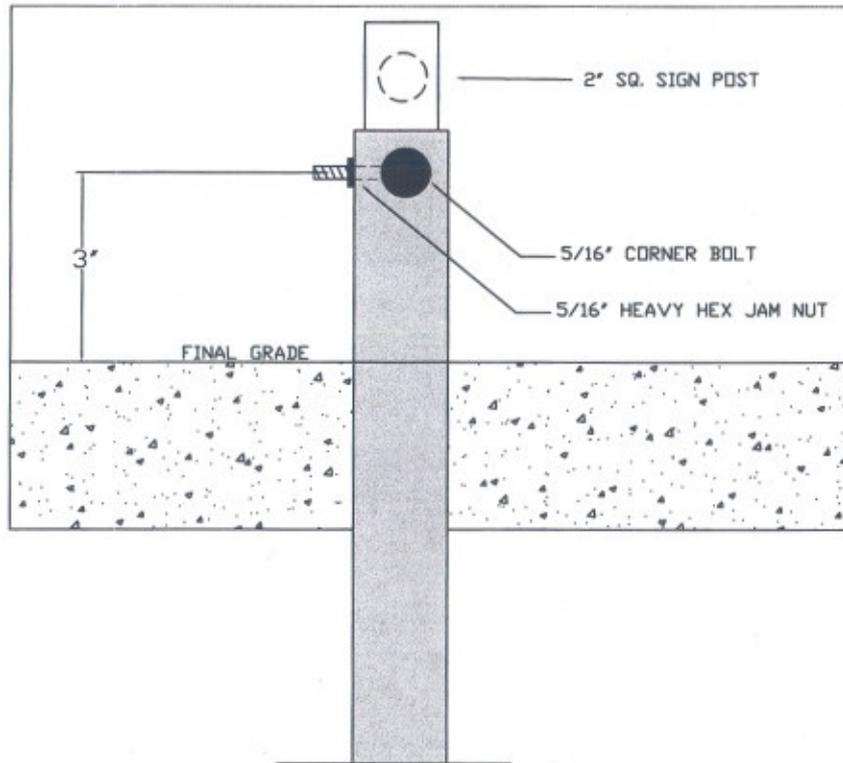
- A. 2" SQUARE POST WITH 7/16" QWIK-PUNCHED HOLES (QWIK-PUNCH TYPE OR EQUIVALENT), 1" CENTER, ON ALL FOUR SIDES FOR STANDARD SIGN SIZES.
- B. LARGER POST (2-3/16" OR 2-1/2") SIZES MAY BE REQUIRED IN COMPARISON TO SIGN SIZE.
- C. HOT DIPPED GALVANIZED 14 GAUGE STEEL WITH A CLEAR POLYMER COATING.
- D. CONFORM TO THE STANDARD SPECIFICATIONS FOR STRUCTURAL QUALITY ASTM DESIGNATION A570, GRADE 50.

(2) POST ANCHOR:

- A. GALVANIZED 7 GAUGE HEAVY DUTY 30° ANCHOR. HOLE MUST BE A MINIMUM OF 12" DIAMETER, ANCHOR MUST BE PLUMB AND LEVEL, SET AT 3" ABOVE FINAL GRADE. HUBS INSTALLED INTO SIDEWALKS NEED ONLY HALF THE AMOUNT OF CONCRETE SET INTO THE HOLE. BOTTOM OF ANCHOR MUST BE CAPPED OR TAPED TO PREVENT CONCRETE AND DIRT ENTERING FROM BOTTOM.
- B. MINIMUM POST DEPTH INSIDE OF ANCHOR MUST BE 12".

(3) SIGN HARDWARE:

- A. 3/8" STEEL DRIVE RIVET W/ 3/8" X 1" NYLON WASHER.
- B. 0.030" X 3/4" STAINLESS STEEL STRAPPING.
- C. 3/4" BAND-IT BUCKLES.
- D. GALVANIZED 5/16" CORNER BOLT WITH 5/16" HEAVY HEX JAM NUT.
- E. SIGN MOUNTING BRACKET: 2" SQ. 90 DEGREE CROSS PIECE (USE LONG SLOT 5-1/4" W/ ALLEN HEAD SET SCREWS)



CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

SIGN POST INSTALATION
ANCHOR DETAIL
(NOT TO SCALE)

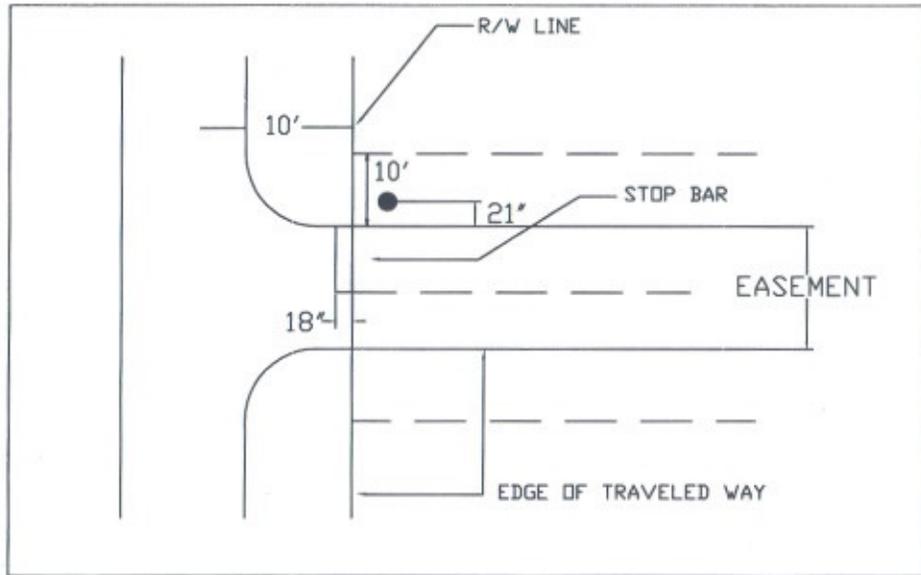
Fig. 2-36

Approval Date:

June 1, 2009

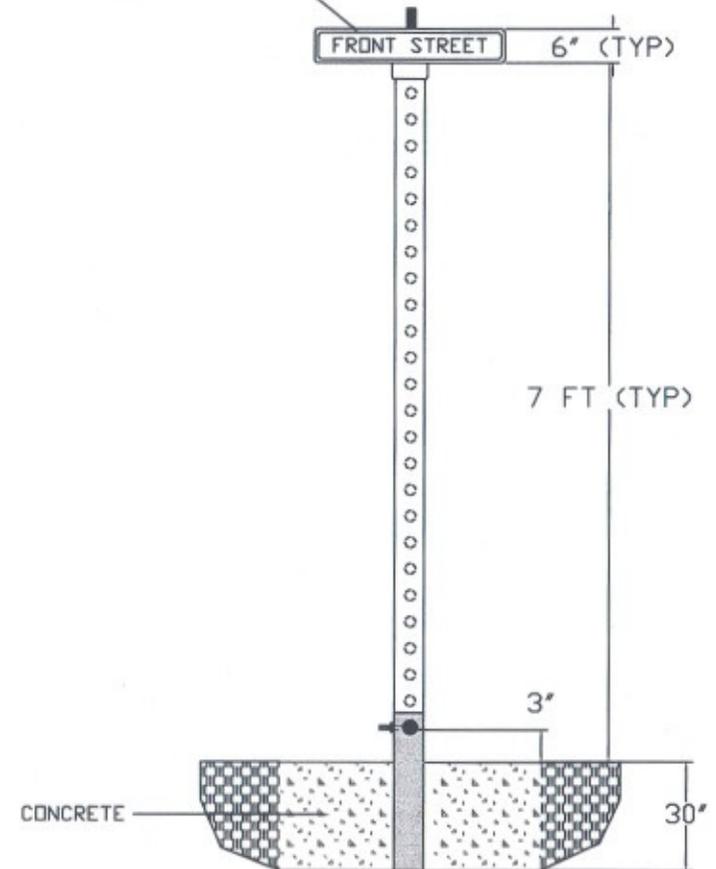
NOTES:

- A. BOTTOM OF SIGNS ARE 7 FT. FROM GRADE.
- B. STOP SIGNS SHALL BE HIGH INTENSITY GRADE SHEETING AND 30" MIN AT SPEEDS LESS THAN 25 M.P.H..
- C. STREET NAME SIGN STANDARD HEIGHT IS 6 INCHES.
- D. HOT DIPPED GALVANIZED 14 GAUGE STEEL WITH A CLEAR POLYMER COATING.
- E. STREET SIGNS WILL BE WHITE LETTERING ON GREEN BACKGROUND.
- F. SIGNS ARE TO BE "FLAT BLADE" TYPE WITH WHITE BORDER.
- G. LETTER/NUMBER STYLE WILL BE SERIES C, 4 INCHES HEIGHT/HI-WAY GOTHIC STYLE FONT.
- H. ENGINEER GRADE, REFLECTORIZED SHEETING.
- I. OTHER HEIGHTS/LENGTHS MAYBE REQUIRED.



SIGN PLACEMENT

SERIES C, 4"
HEIGHT/HI-WAY
GOTHIC SYLE FONT



CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

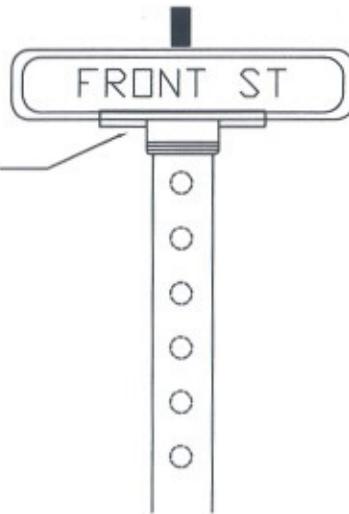
SIGN PLACEMENT
PRIVATE / PUBLIC RD.
(NOT TO SCALE)

Fig. 2-37

Approval Date:

June 1, 2009

SIGN MOUNTING BRACKET:
2" SQ. 90 DEGREE CROSS
PIECE (LONG SLAT 5-1/4"
ALLEN HEAD SET SCREWS)



ABBREVIATIONS:

STREET =	ST
AVENUE =	AVE
PLACE =	PL
WAY =	WAY
BOULEVARD =	BLVD
PARKWAY =	PKWY
LANE =	LN
COURT =	CT
DRIVE =	DR
ROAD =	RD
KEY =	KEY
CONNECTOR =	CONN

STREET NAME SIGN DETAIL



- NOTES:
- A. STREET NAME SIGN STANDARD HEIGHT IS 6 INCHES.
 - B. LETTER/NUMBER STYLE WILL BE SERIES C, 4 INCHES HEIGHT/HI-WAY GOTHIC STYLE FONT.
 - C. SIGNS ARE TO BE "FLAT BLADE" TYPE WITH WHITE BORDER.
 - D. ENGINEER GRADE, REFLECTORIZED SHEETING.
 - E. OTHER HEIGHTS/LENGTHS MAY BE REQUIRED.
 - F. SIGNS 36" OR LONGER REQUIRE 12" SLOT.



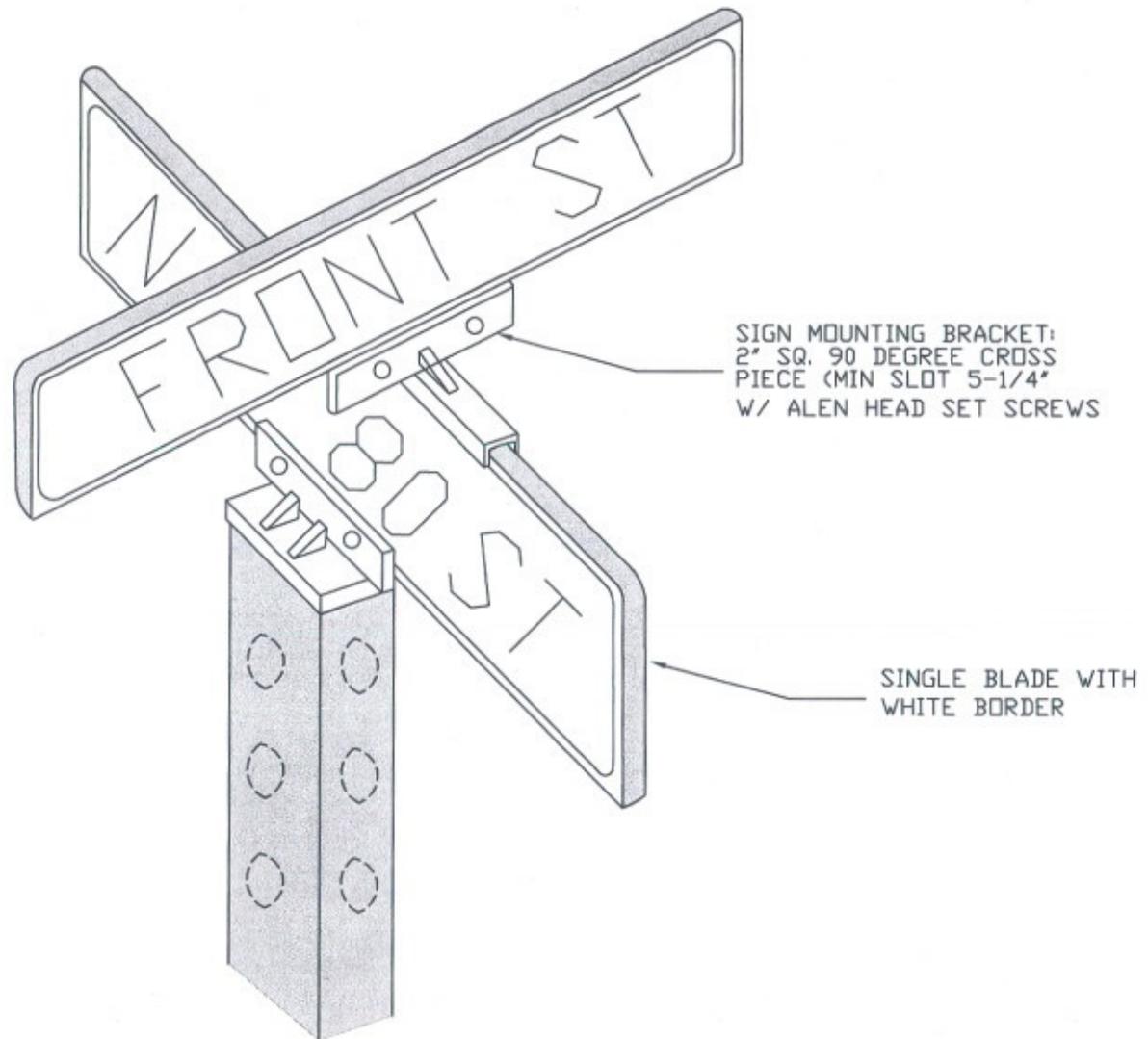
CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

SIGN FABRICATION DETAIL
(NOT TO SCALE)

Fig. 2-38

Approval Date:

June 1, 2009



SEE DWG. 1-01 FOR DETAIL OF TYPICAL POST INSTALATION

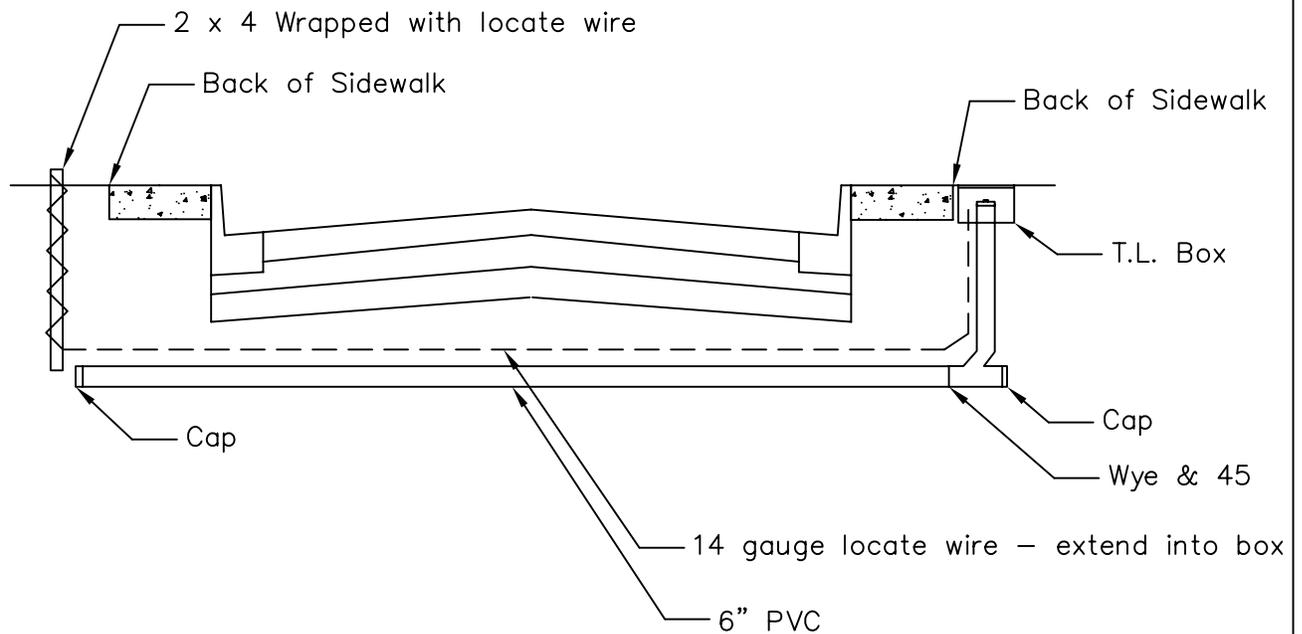


CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

SIGN STANDARDS
(NOT TO SCALE)

Fig. 2-39

Approval Date:
June 1, 2009



1. The Conduit location(s) will be staked in the field by the Engineer. The conduit material shall be 6" PVC. The West side shall be brought out to finished grade and placed in a Fogtite B-9 T.L. box or approved equal. The East side shall be marked with a treated 2x4 wrapped with the locate wire. The board shall be cut off 6" above finished grade.
2. Minimum conduit cover (burial depth) shall be 3 feet. Actual cover depth will vary due to existing utilities, and shall be as determined by the City Engineer or City Inspector.



CITY OF POULSBO
DEPARTMENT OF PUBLIC WORKS

TYPICAL ROADWAY CONDUIT
CROSSING DETAIL

REVISED BY: MJB
DATE: 5/12/09

APPROVED BY: _____
DATE: June 1, 2009

Fig. 2-50