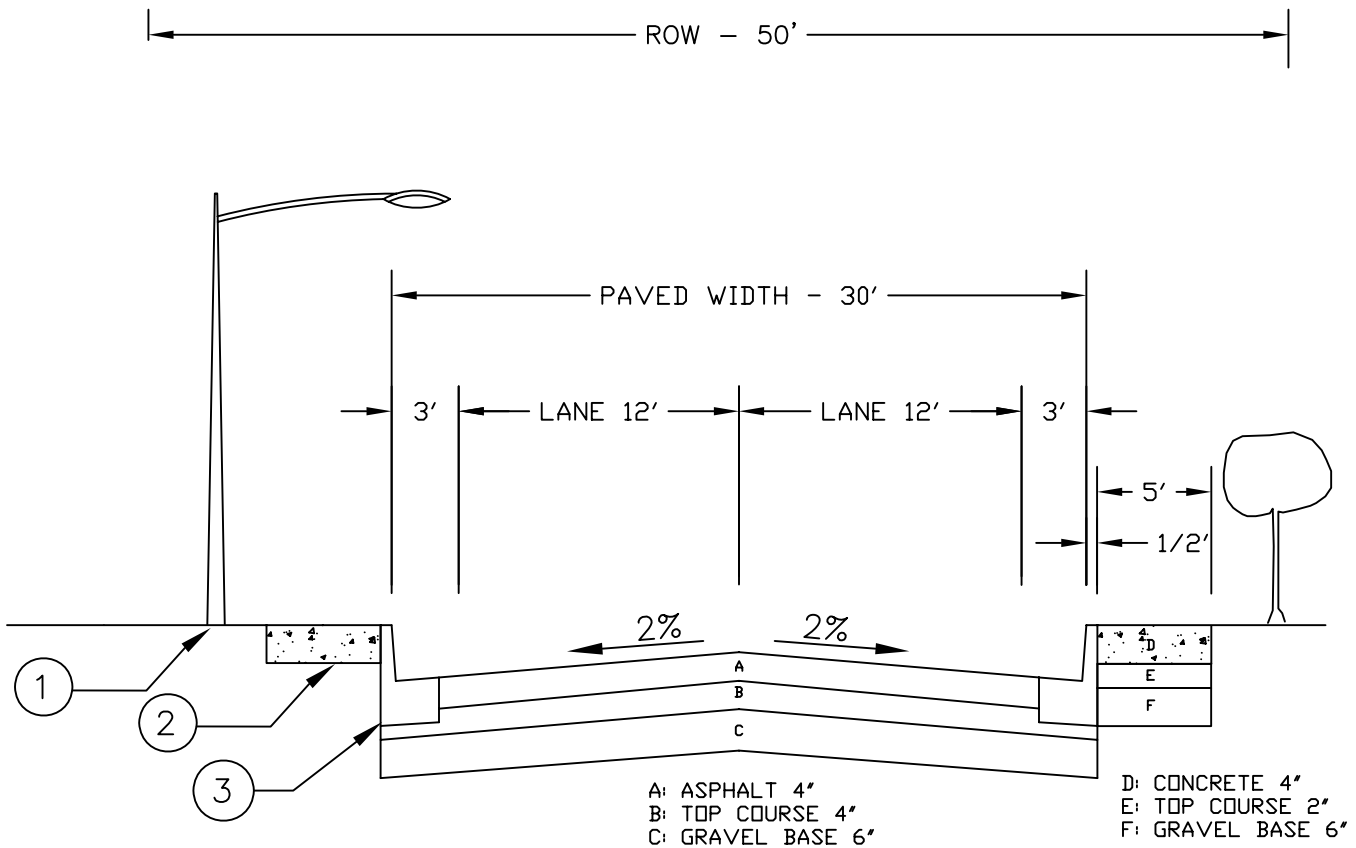


Design Standard	Neighborhood/ Commercial Collector	Residential Collector	Residential Access	Commercial Access
<b>Figure</b>	2-02	2-03	2-04	2-05
<b>ROW Width</b>	50' two lanes 60' three lanes	50'	40'; or 45' if sidewalks required on both sides	As required
<b>Pavement Width</b>	30' two lanes 42' three lanes	30'	28'	24'
<b>Recommended ADT <sup>1</sup></b>	> 4000	400 - 4000	< 400	< 1000
<b>Sidewalk</b>	Both sides	Both sides	One side; both sides if required by City Engineer	As required by City Engineer
<b>Lane width</b>	12'	11'	10'	12'
<b>Paved shoulder</b>	3'	none	none	none
<b>Connectivity</b>	Yes	Yes	No	No
<b>Maximum grade</b>	12%	12%	12%	12%
<b>Curb radii</b>	35'	35'	25'	35'
<b>On-street Parking</b>	None unless 8' bulb-outs provided	8' one side	8' one side	None unless 8' bulb-outs provided

**Figure 2-01. Local Access Streets**

<sup>1</sup> Volumes based on 9.55 Average Daily Trips (ADT) per residential unit.



- ① PLANTING, LANDSCAPING AND STREET LIGHTING STRIP, BOTH SIDES
- ② CEMENT CONCRETE SIDEWALK, 5' - BOTH SIDES
- ③ CEMENT CONCRETE CURB & GUTTER (SEE WSDOT STD. PLANS)

## TYPICAL ROADWAY SECTION NEIGHBORHOOD/COMMERCIAL COLLECTOR N.T.S.



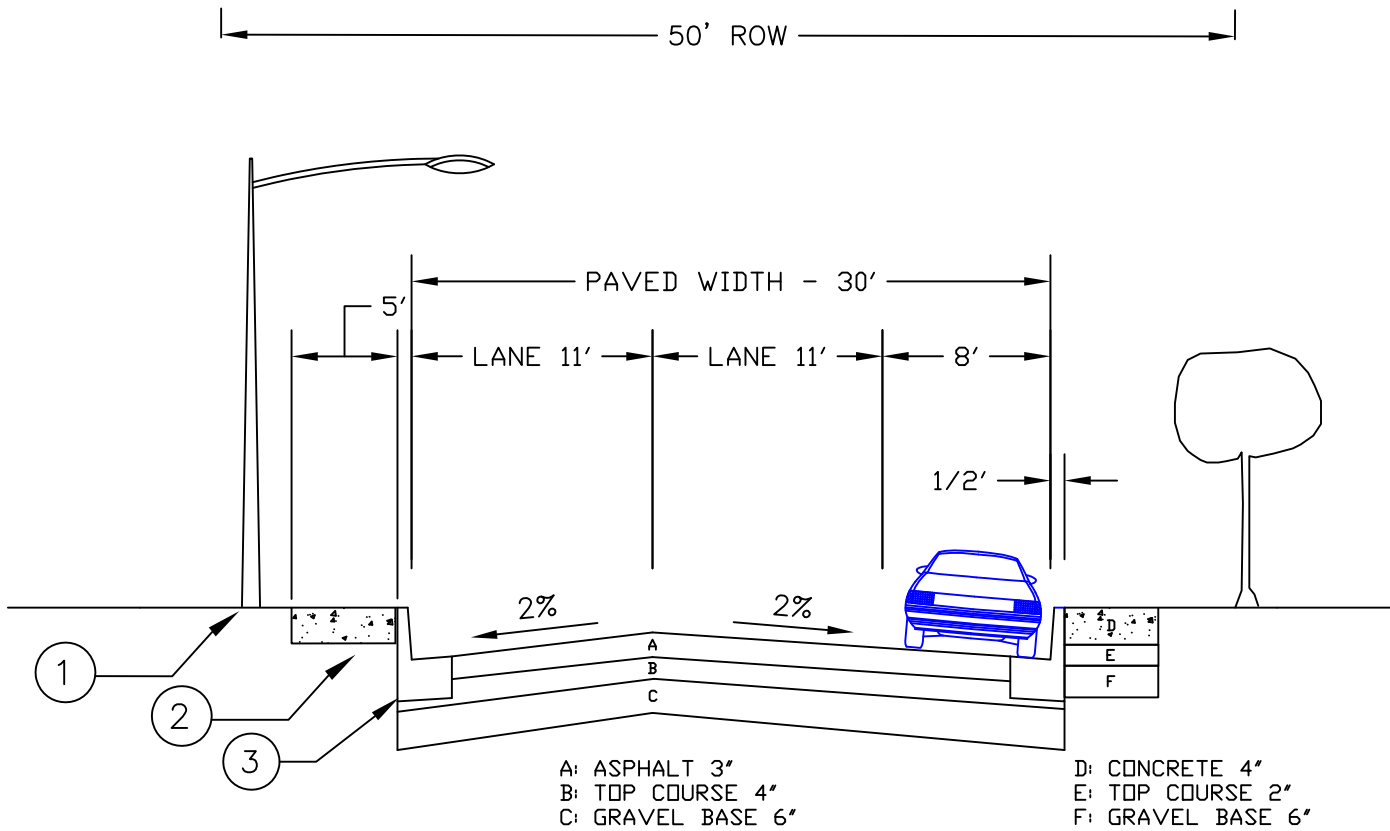
CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

TYPICAL ROADWAY SECTION  
NEIGHBORHOOD/COMMERCIAL  
COLLECTOR

REVISED BY: RDF  
DATE: 10/13/08

APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

Fig. 2-02



- ① PLANTING, LANDSCAPING AND STREET LIGHTING STRIP, BOTH SIDES
- ② CEMENT CONCRETE SIDEWALK, 5' - BOTH SIDES
- ③ CEMENT CONCRETE CURB & GUTTER (SEE WSDOT STD. PLANS)

## TYPICAL ROADWAY SECTION RESIDENTIAL COLLECTOR N.T.S.



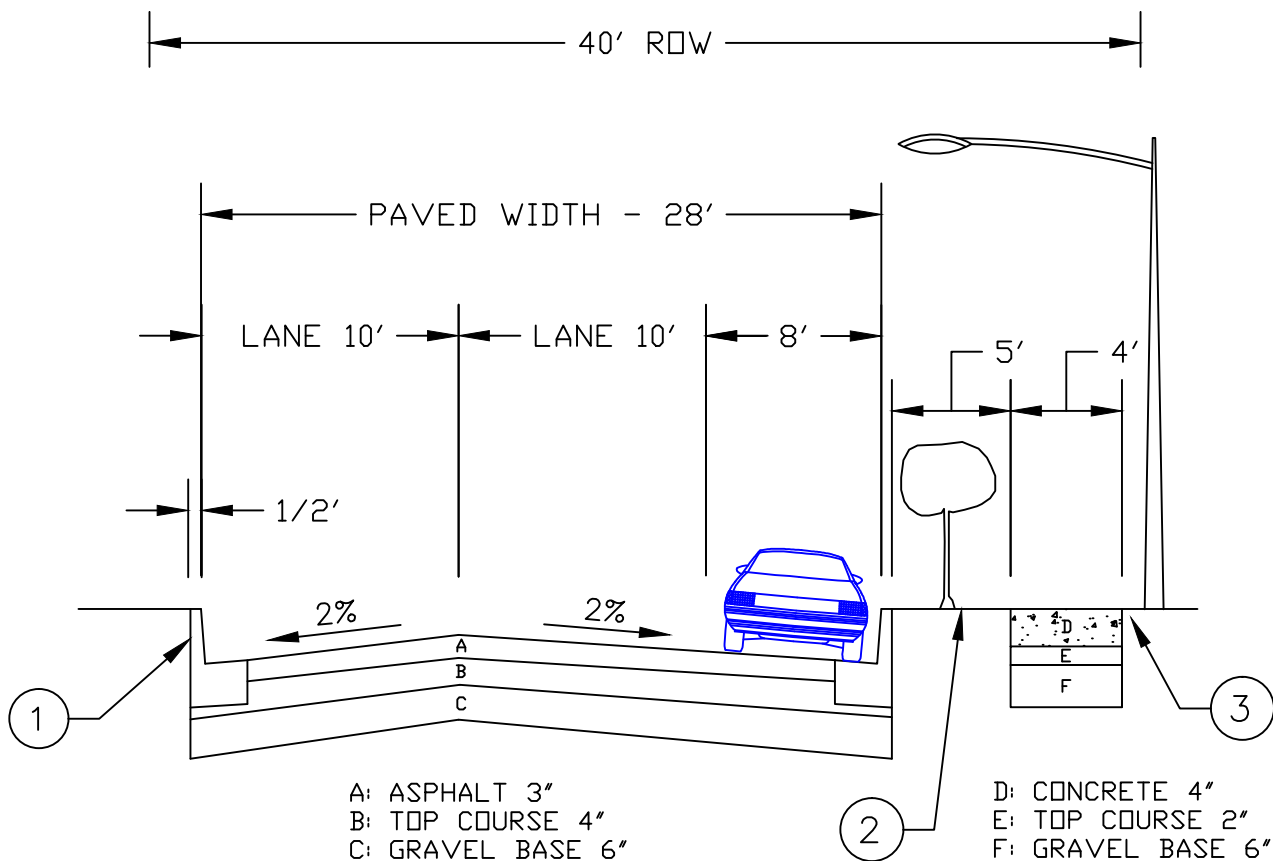
CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

TYPICAL ROADWAY SECTION  
RESIDENTIAL COLLECTOR

REVISED BY: RDF  
DATE: 10/13/08

APPROVED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

Fig. 2-03



- ① CEMENT CONCRETE CURB & GUTTER (SEE WSDOT STD. PLANS)
- ② PLANTING, LANDSCAPING AND STREET LIGHTING STRIP, ONE SIDE
- ③ CEMENT CONCRETE SIDEWALK, 4' - ONE SIDE. SIDEWALK SHALL BE 5' WIDE IF NOT SEPERATED FROM TRAVEL LANES BY PARKING STRIP, LANDSCAPING, BIOSWALE, OR RAINGARDENS.

TYPICAL ROADWAY SECTION  
 RESIDENTIAL ACCESS  
 DETACHED SIDEWALK, ONE SIDE  
 N.T.S.



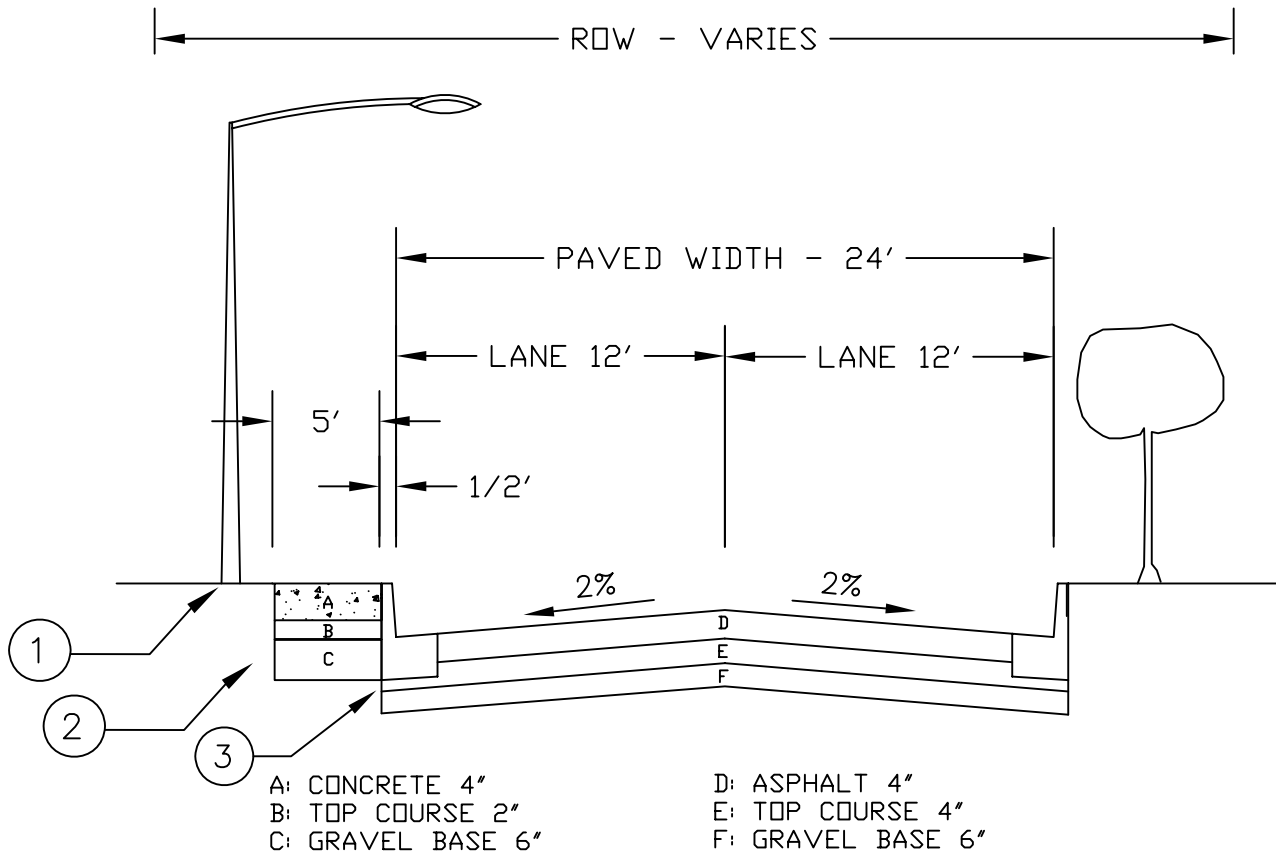
CITY OF POULSBO  
 DEPARTMENT OF PUBLIC WORKS

TYPICAL ROADWAY SECTION  
 RESIDENTIAL ACCESS  
 SIDEWALK ONE-SIDE

REVISED BY: RDF  
 DATE: 10/13/08

APPROVED BY: \_\_\_\_\_  
 DATE: June 1, 2009

Fig. 2-04  
 REF: P-yyy



- ① PLANTING, LANDSCAPING AND STREET LIGHTING STRIP, BOTH SIDES
- ② CEMENT CONCRETE SIDEWALK, 5' - AS REQUIRED
- ③ CEMENT CONCRETE CURB & GUTTER (SEE WSDOT STD. PLANS)

TYPICAL ROADWAY SECTION  
COMMERCIAL ACCESS  
SIDEWALK ONE SIDE

N.T.S.



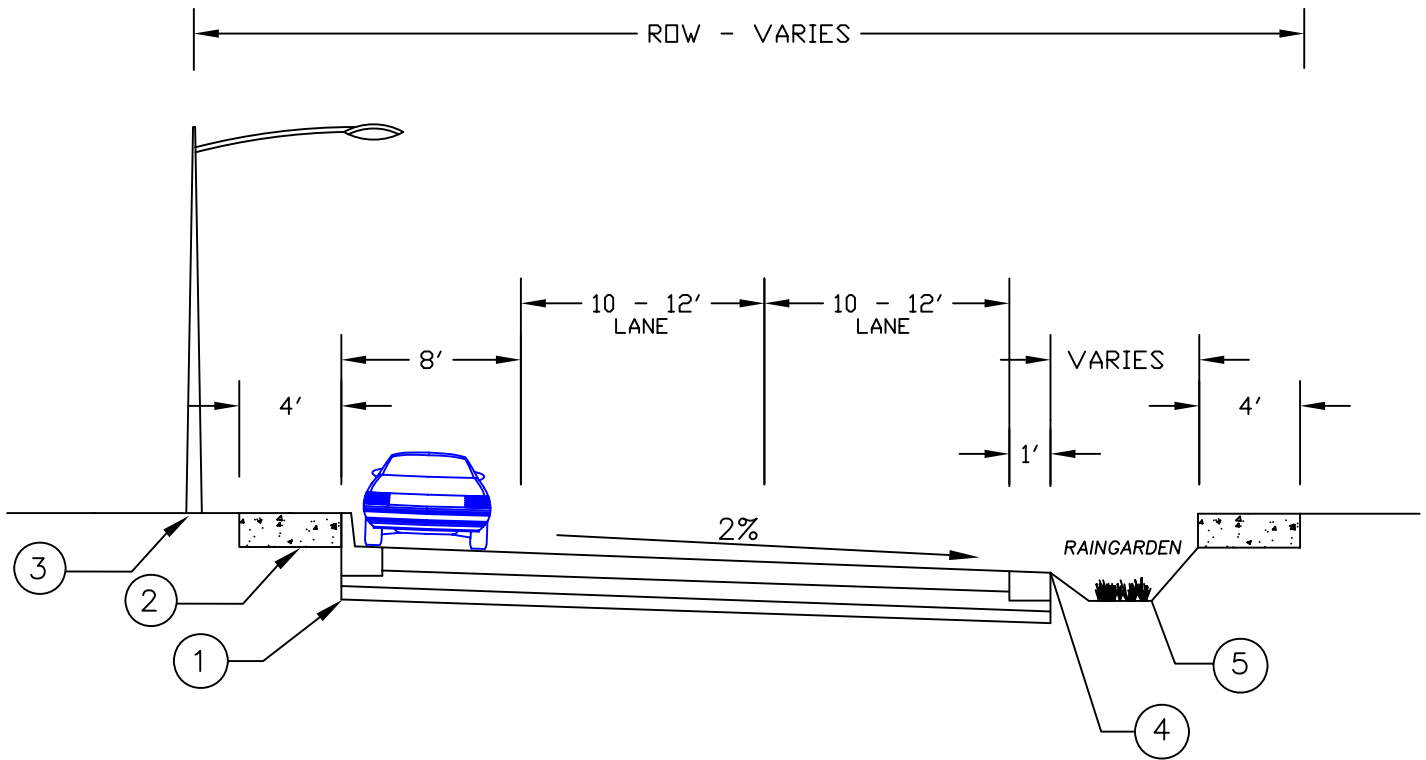
CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

TYPICAL ROADWAY SECTION  
COMMERCIAL ACCESS  
SIDEWALK ONE SIDE

REVISED BY: RDF  
DATE: 10/13/08

APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

Fig. 2-05  
REF: P-yyy



- ① CEMENT CONCRETE CURB & GUTTER (SEE WSDOT STD. PLANS)
- ② CEMENT CONCRETE SIDEWALK, 4'
- ③ PLANTING, LANDSCAPING AND STREET LIGHTING STRIP, BOTH SIDES
- ④ FLUSH CEMENT CONCRETE CURB
- ⑤ RAINGARDEN IN ACCORDANCE WITH STORMWATER DESIGN STANDARDS

## TYPICAL ROADWAY SECTION LOW IMPACT DEVELOPMENT N.T.S.

NOTE: LID TECHNIQUES MAY BE USED ON ANY LOCAL ACCESS STREET  
DESCRIBED IN FIGURE 2-01 PER THE DISCRETION OF THE CITY ENGINEER



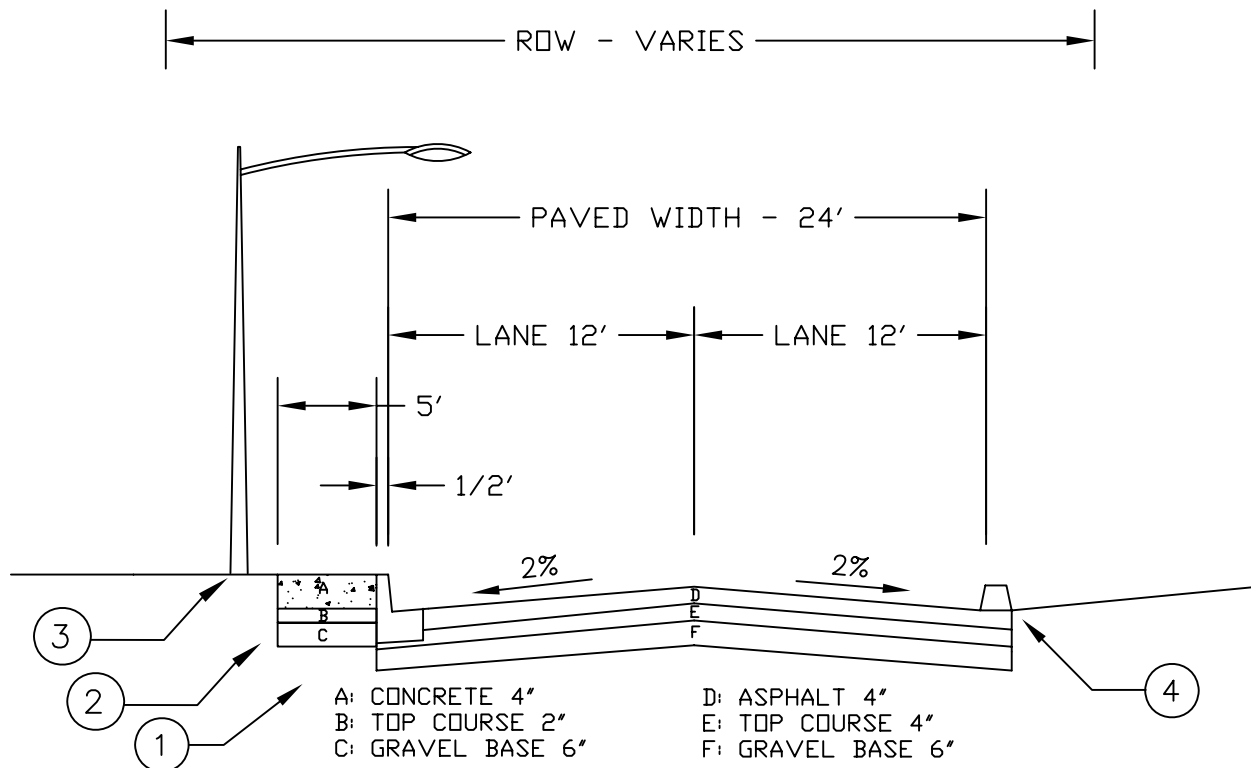
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DEPARTMENT OF PUBLIC WORKS

TYPICAL ROADWAY SECTION  
LOW IMPACT DEVELOPMENT

REVISED BY: RDF  
DATE: 10/13/08

APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

Fig. 2-08  
REF: P-yyy



- ① CEMENT CONCRETE CURB & GUTTER (SEE WSDOT STD. PLANS)
- ② PORTLAND CEMENT CONCRETE SIDEWALK, 5'- ONE SIDE
- ③ PLANTING, LANDSCAPING AND STREET LIGHTING STRIP, ONE SIDE
- ④ TYPE 3 EXTRUDED ASPHALT CURB (SEE WSDOT STD. PLANS)
- ⑤ ON-STREET PARKING MAY BE REQUIRED IF PARKING REQUIREMENTS ARE NOT SATISFIED ELSEWHERE ON-SITE. IF PARKING IS REQUIRED, THE PAVED WIDTH WILL BE 28 FEET.

## TYPICAL ROADWAY SECTION HALF-WIDTH STREET IMPROVEMENT SIDEWALK ONE SIDE

N.T.S.

ALL HALF-WIDTH STREET IMPROVEMENTS WILL BE ASSESSED ON A CASE-BY-CASE BASIS BY THE CITY ENGINEER. THIS STREET SECTION MAY NOT BE APPROPRIATE FOR ALL HALF-WIDTH STREET IMPROVEMENTS.



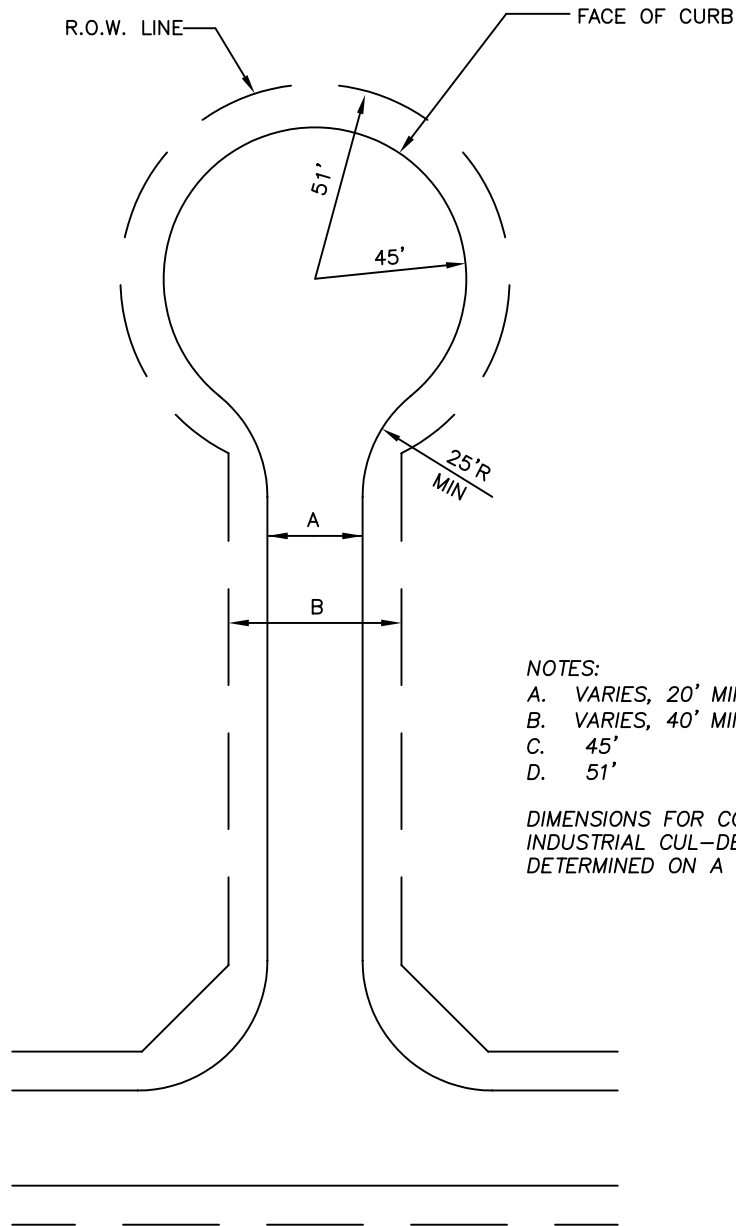
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DEPARTMENT OF PUBLIC WORKS

TYPICAL ROADWAY SECTION  
HALF-WIDTH STREET IMPROVEMENT  
SIDEWALK ONE SIDE

REVISED BY: RDF  
DATE: 10/13/08

APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

Fig. 2-09  
REF: P-yyy



NOTES:

- A. VARIES, 20' MINIMUM
- B. VARIES, 40' MINIMUM, SEE FIGURE 2-01
- C. 45'
- D. 51'

DIMENSIONS FOR COMMERCIAL AND INDUSTRIAL CUL-DE-SACS WILL BE DETERMINED ON A CASE-BY-CASE BASIS.

THE MAXIMUM LENGTH OF A CUL-DE-SAC SHALL NOT EXCEED 500'.

TYPICAL CUL-DE-SAC

N.T.S.



CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

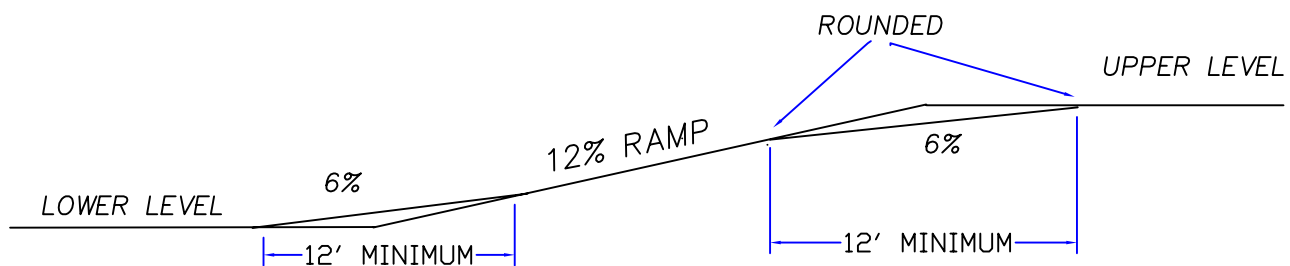
RESIDENTIAL CUL-DE-SAC

REVISED BY: JAL  
DATE: 5/2/2005

APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

Fig. 2-10  
REF: P-307





TRANSITION REQUIRED WHEN  
ALGEBRAIC DIFFERENCE OF  
GRADES EXCEEDS 6%

NOTES:  
This type of transition must be constructed  
for all ramps between floors, parking areas,  
streets, etc., to allow adequate clearances  
for long overhang vehicles.



CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

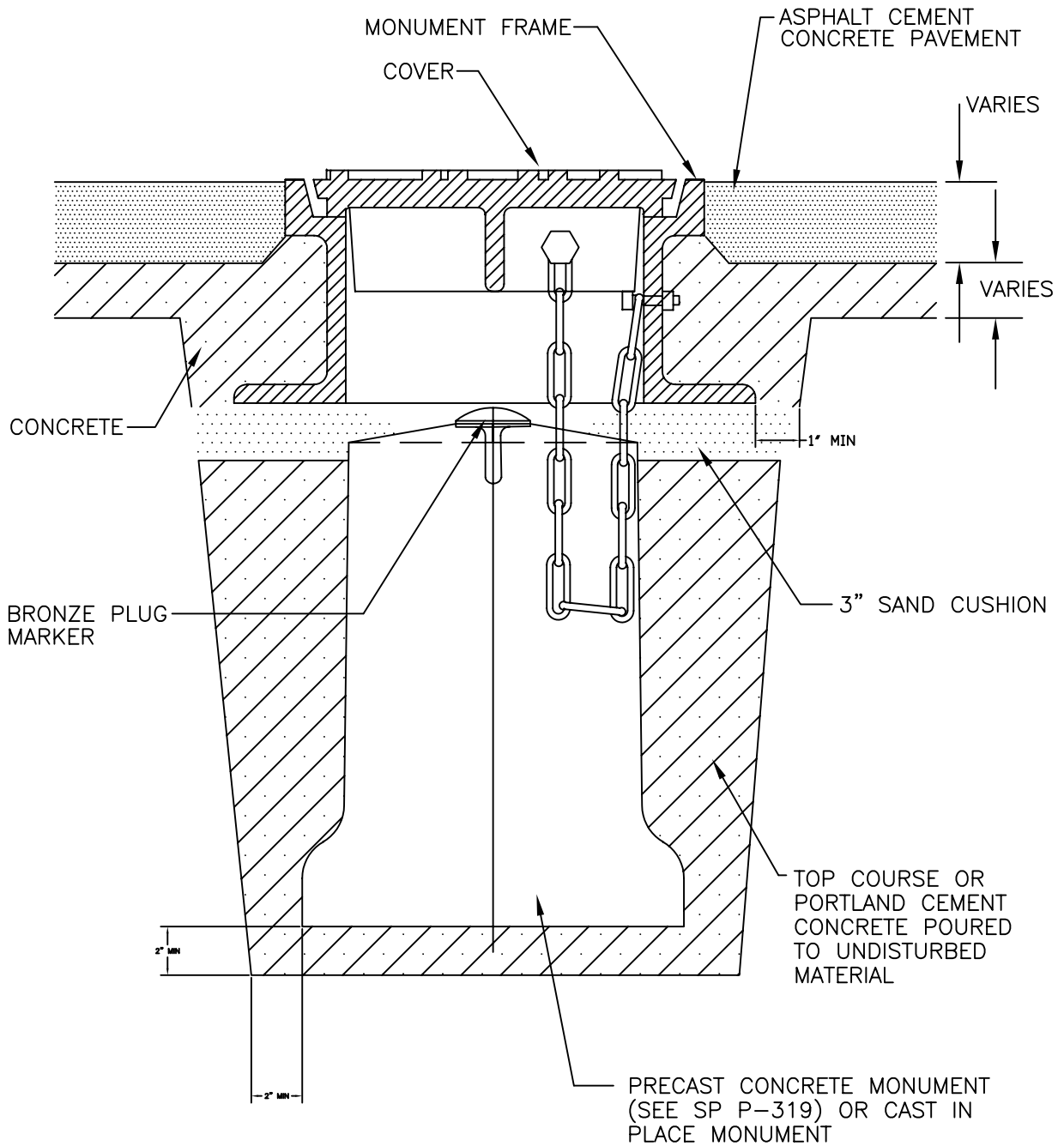
GRADE TRANSITION

REVISED BY: JAL  
DATE: 3/28/2005

APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

Fig. 2-14  
Ref: Poulsbo H-10

REVISED: 3/28/2005-JAL file: \2-16.dwg



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DEPARTMENT OF PUBLIC WORKS

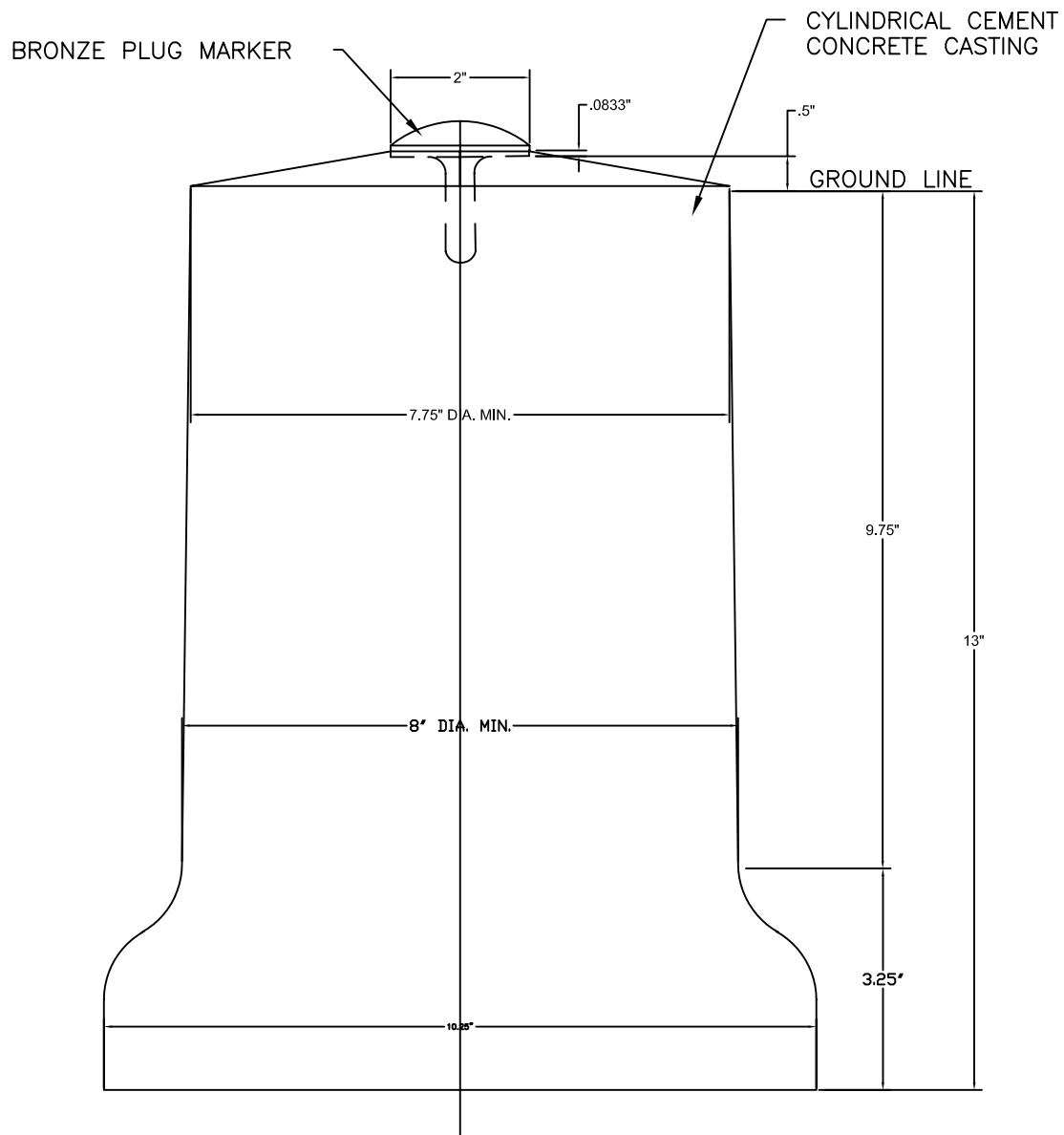
MONUMENT INSTALLATION

REVISED BY: JAL  
DATE: 3/28/2005

APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

Fig. 2-16  
REF: P-318

REVISED: 3/28/2005-JAL file: \2-17.dwg



PRECAST CONCRETE  
MONUMENT

N.T.S.  
CITY DRAWING P-319



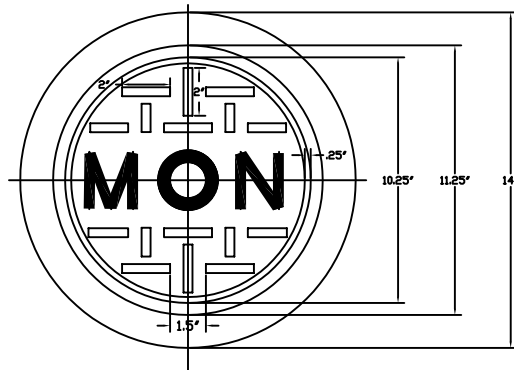
CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

PRECAST CONCRETE MONUMENT

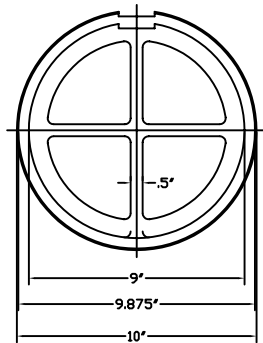
REVISED BY: JAL  
DATE: 3/28/2005

APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

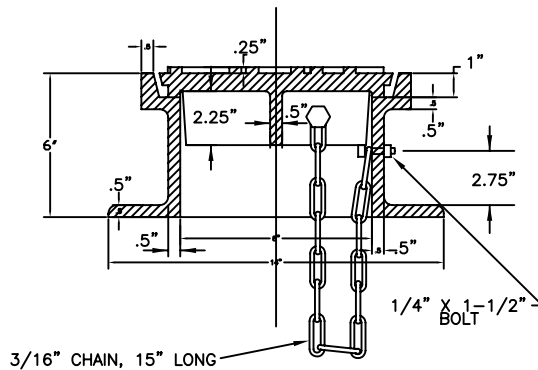
Fig. 2-17  
REF: P-319



TOP VIEW—FRAME & COVER



BOTTOM VIEW OF COVER



SECTION—FRAME &amp; COVER

CONFORMS TO APWA PLAN # 19

MONUMENT  
FRAME & COVER

N.T.S.  
CITY DRAWING B-320



CITY OF POULSBORO  
DEPARTMENT OF PUBLIC WORKS

MONUMENT FRAME AND COVER

REVISED BY: JAL  
DATE: 3/28/2005

APPROVED BY: \_\_\_\_\_  
DATE: **June 1, 2009**

Fig. 2-18  
REF: P-320



CITY OF POULSBORO  
DEPARTMENT OF PUBLIC WORKS

REVISED BY: JAL  
DATE: 3/28/2005

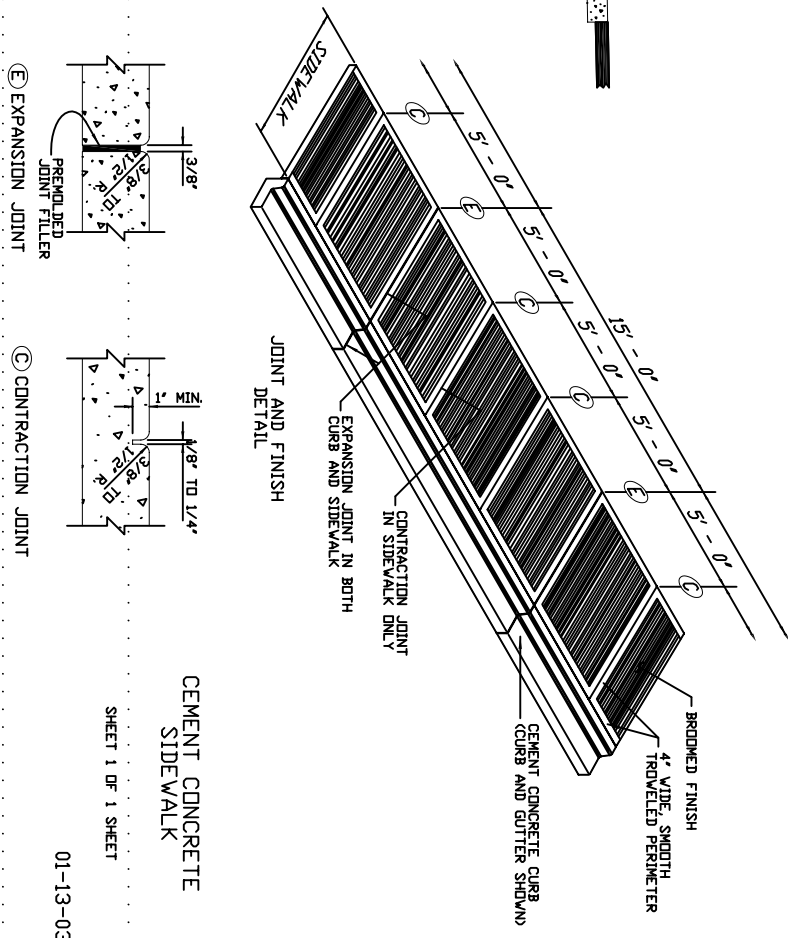
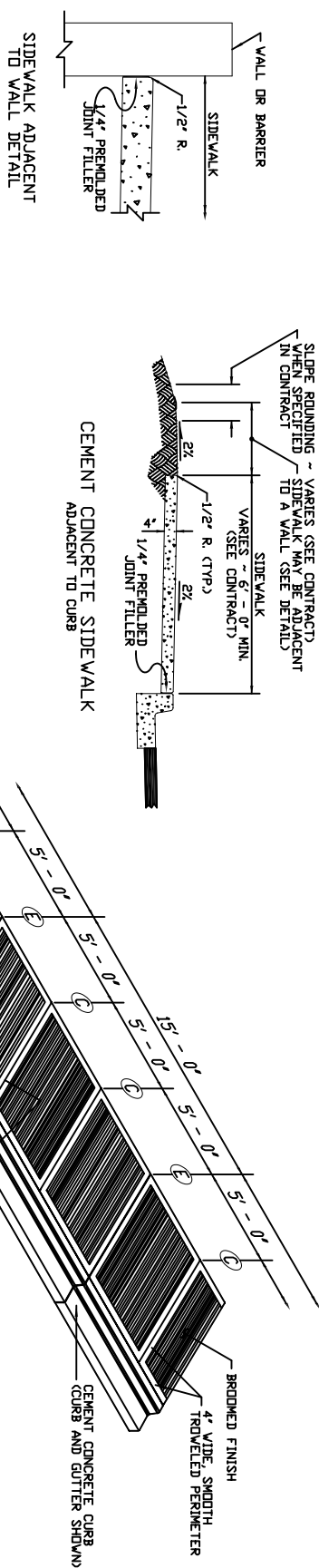
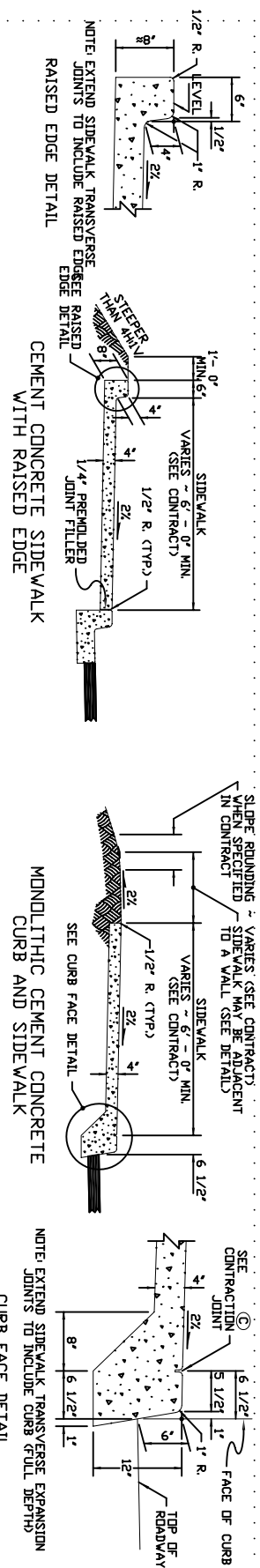
APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

CEMENT CONCRETE SIDEWALK

CEMENT CONCRETE  
SIDEWALK

**SHEET 1 OF 1 SHEET**

01-13-03



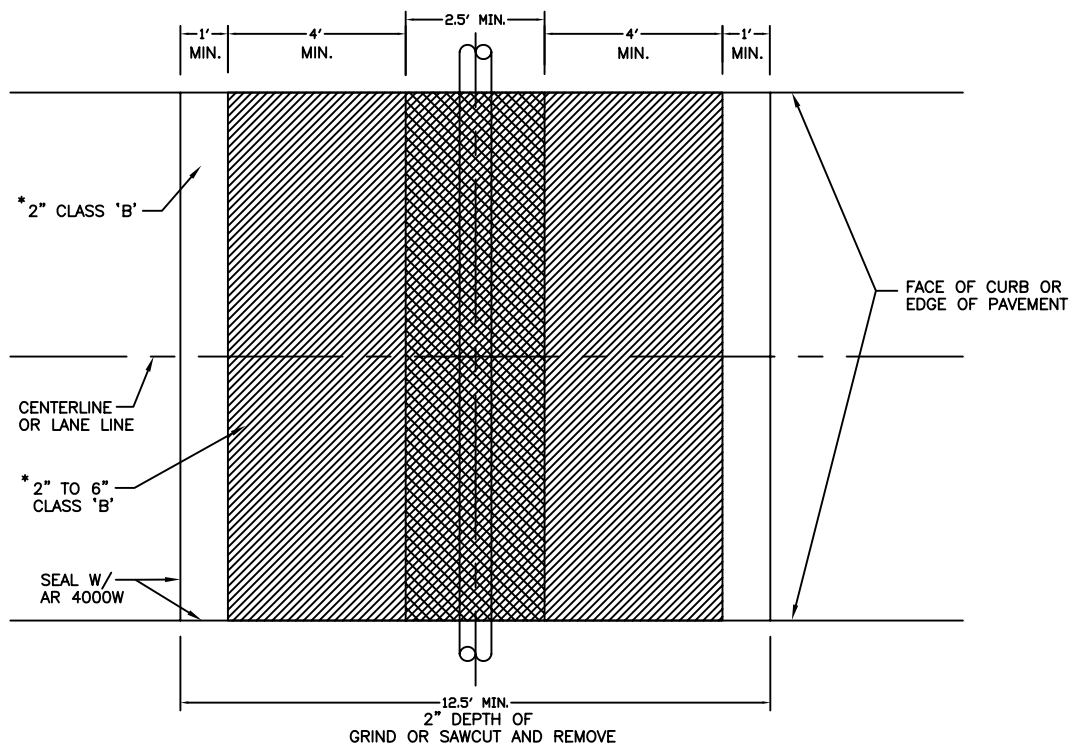
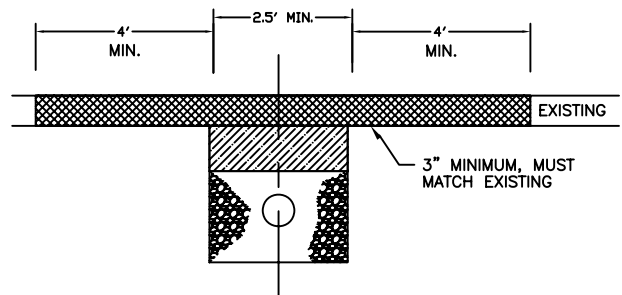
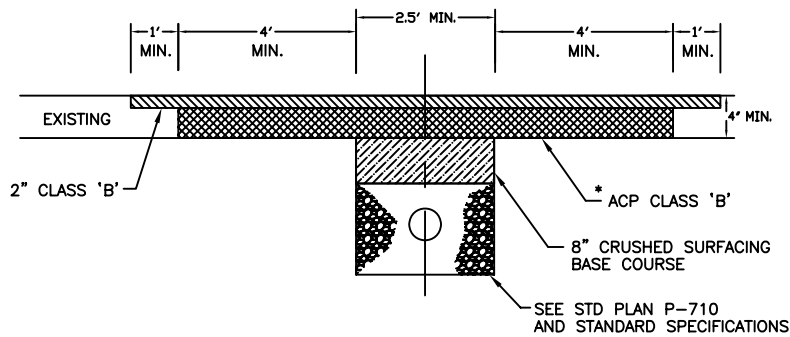
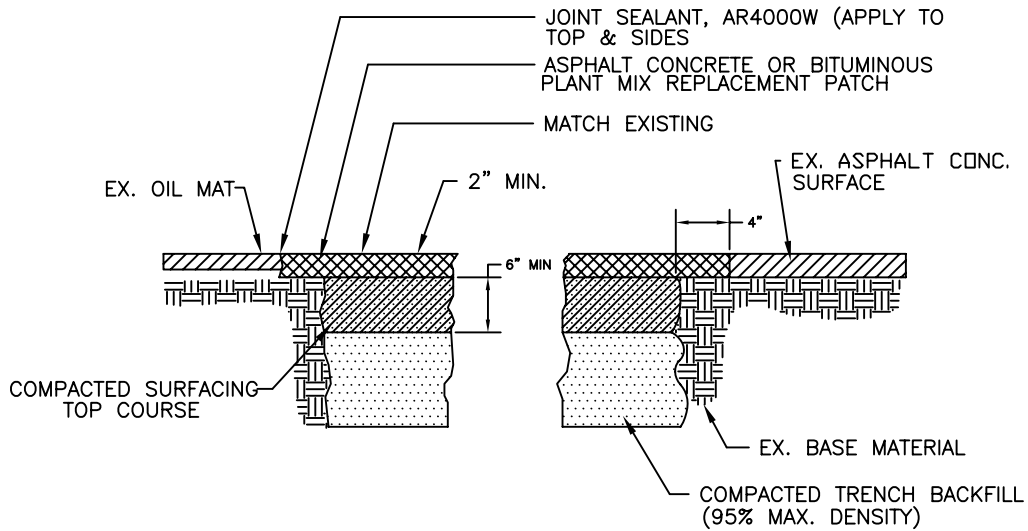
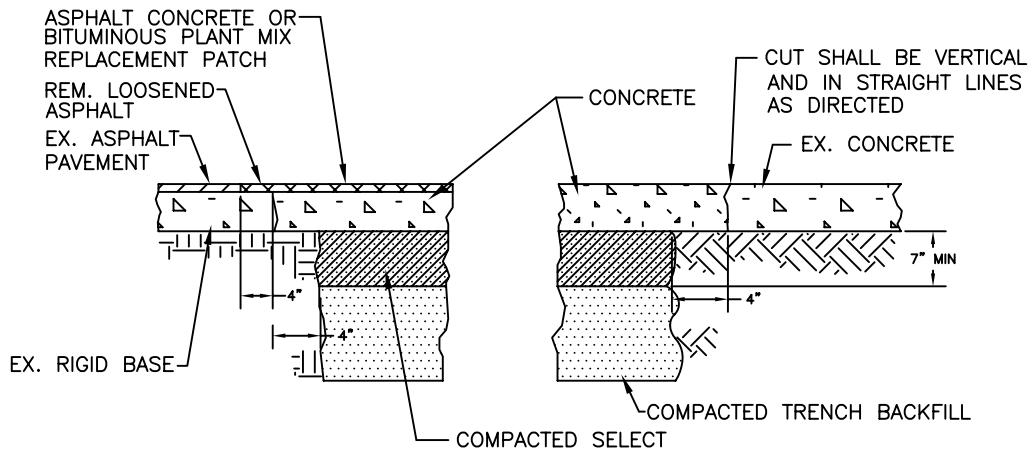


Fig. 2-29  
REF: P-367

REVISED: 3/28/2005-JAL file: \2-30.dwg



TYPICAL ASPHALT CONCRETE  
PAVEMENT PATCH



TYPICAL CEMENT CONCRETE  
PAVEMENT RESTORATION

TYPICAL PAVEMENT RESTORATION  
N.T.S.



CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

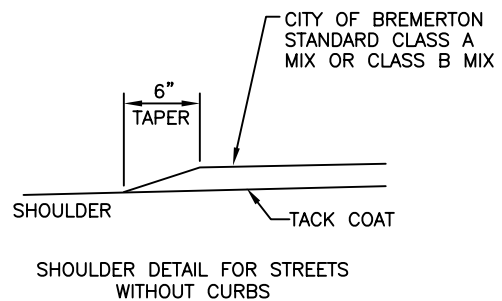
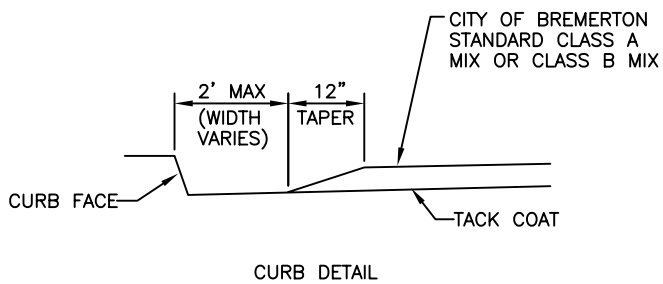
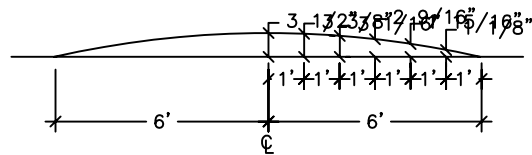
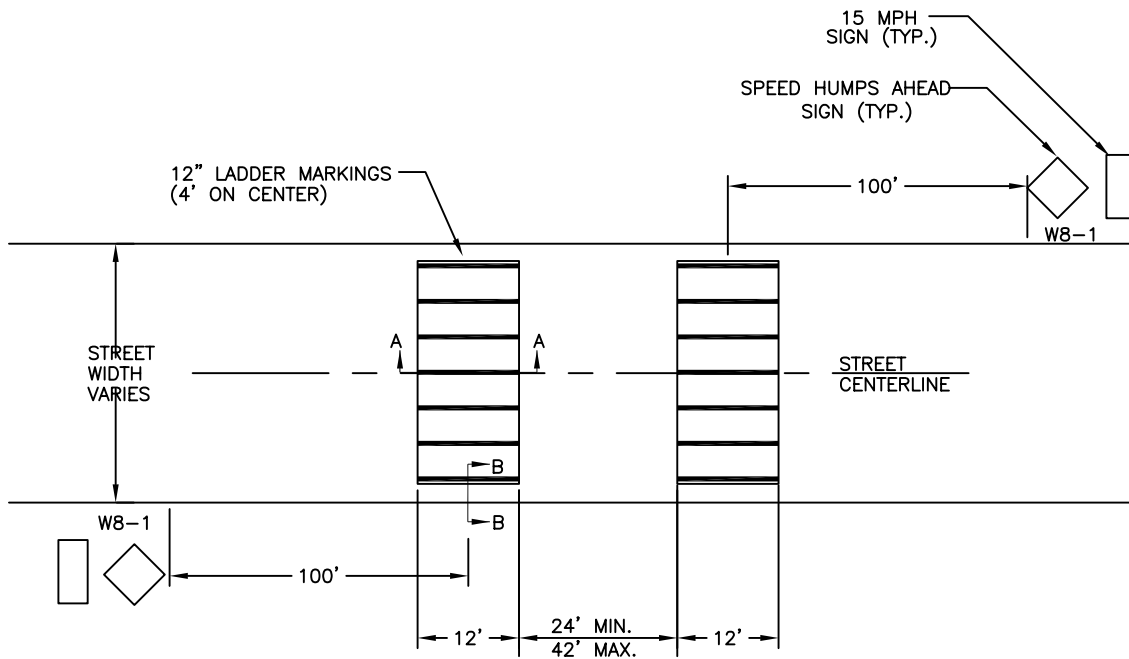
TYPICAL PAVEMENT RESTORATION

REVISED BY: JAL  
DATE: 3/28/2005

APPROVED BY:  
DATE: June 1, 2009

Fig. 2-30  
REF: P-333

REVISED: 3/28/2005-JAL file: \2-31.dwg



SECTION B-B

SPEED HUMP DETAIL

N.T.S.



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DEPARTMENT OF PUBLIC WORKS

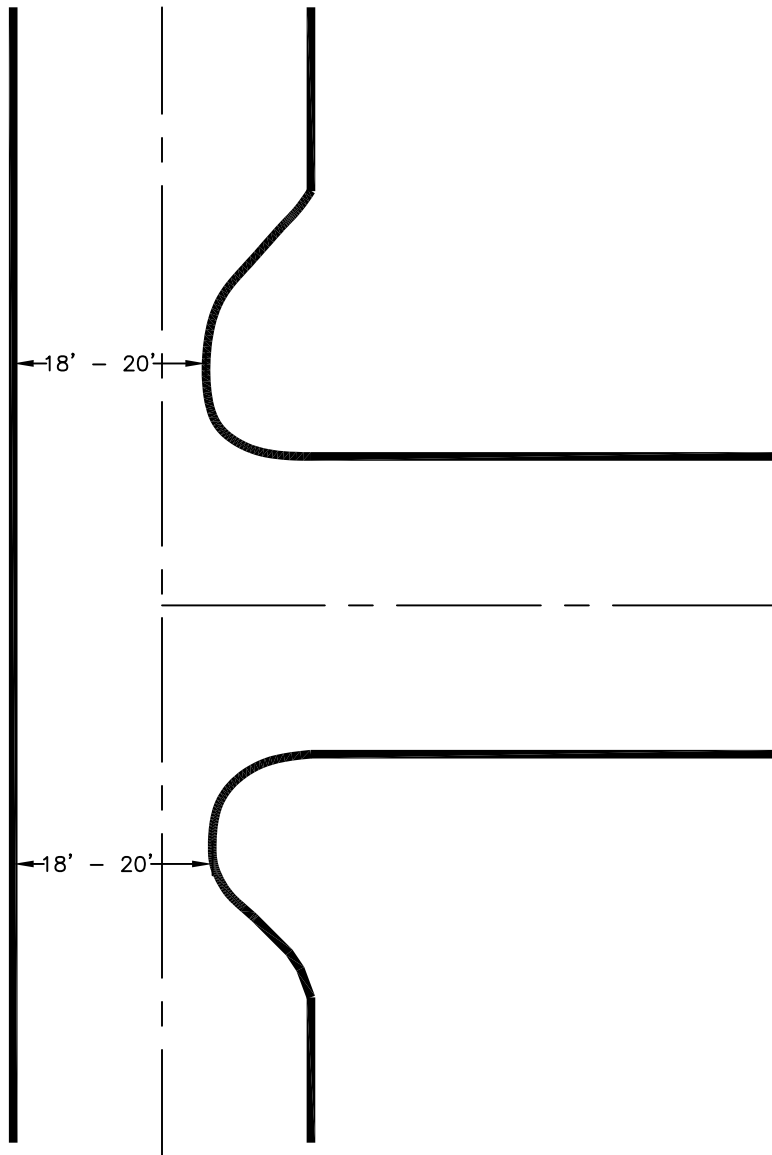
TRAFFIC CALMING  
SPEED HUMP

REVISED BY: JAL  
DATE: 3/28/2005

APPROVED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

Fig. 2-31  
REF: P-371





CURB "BULB OUTS"  
N.T.S.



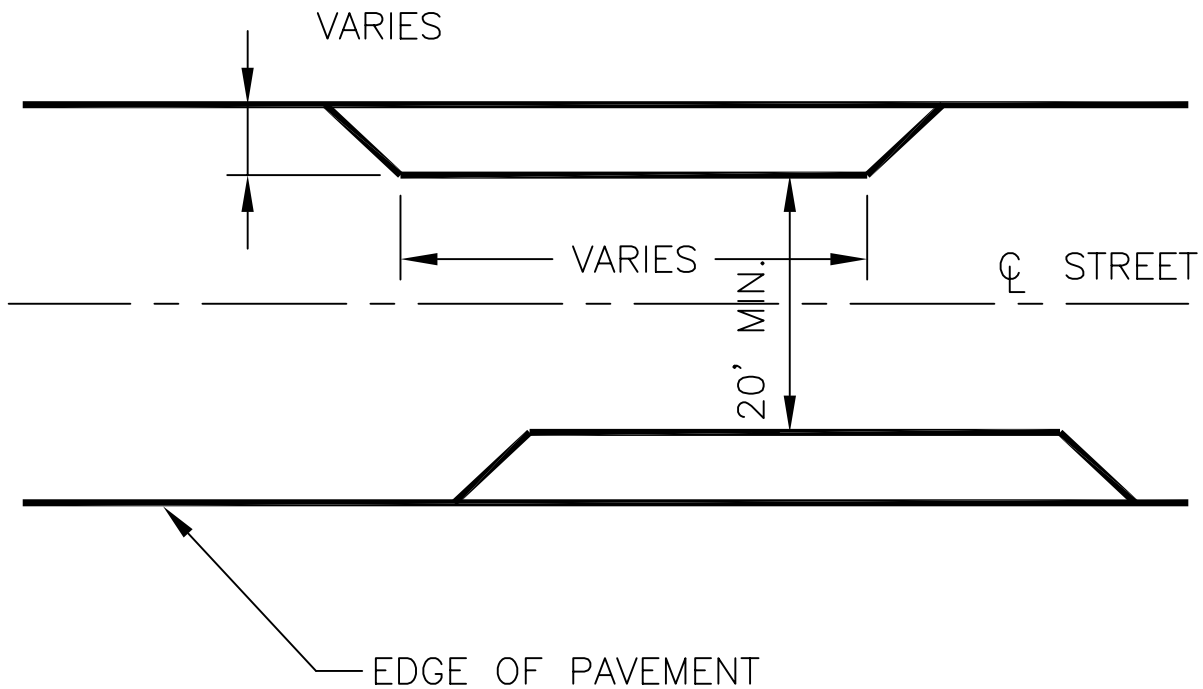
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DEPARTMENT OF PUBLIC WORKS

TRAFFIC CALMING  
BULB-OUT

REVISED BY: JAL  
DATE: 3/28/2005

APPROVED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

Fig. 2-32  
REF: P-372



## TWO-LANE SLOW POINT

N.T.S.  
CITY DRAWING P-373



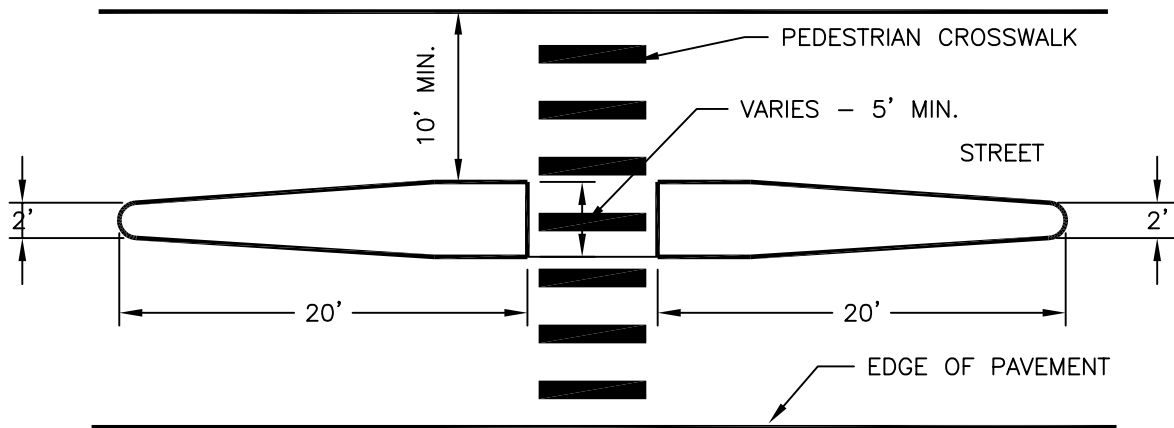
CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

TRAFFIC CALMING  
TWO-LANE SLOW POINT

REVISED BY: JAL  
DATE: 3/28/2005

APPROVED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

Fig. 2-33  
REF: P-373



MID-BLOCK MEDIAN  
N.T.S.



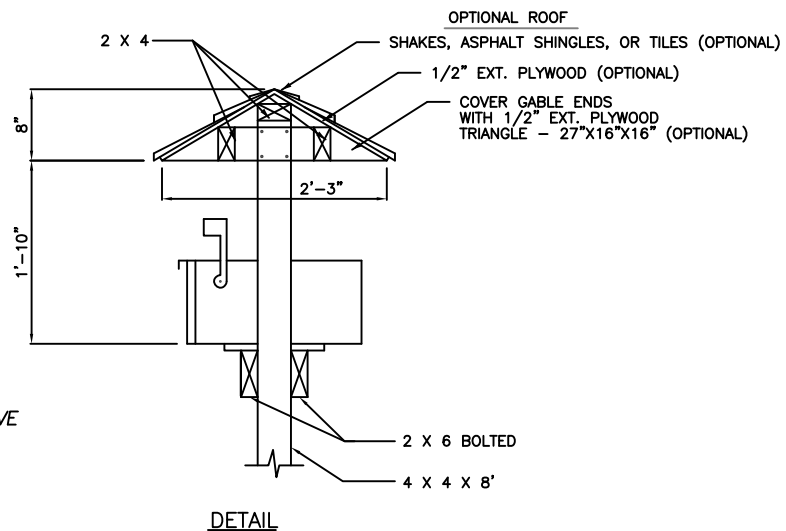
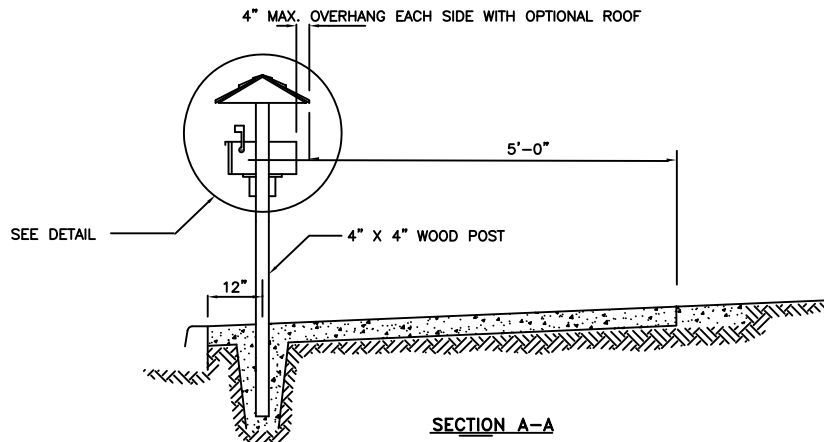
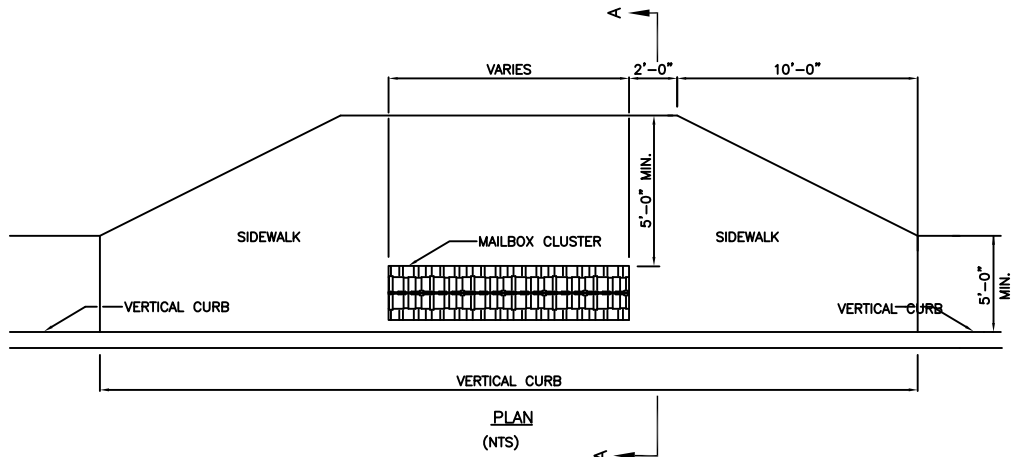
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TRAFFIC CALMING  
MID-BLOCK MEDIAN

REVISED BY: JAL  
DATE: 3/28/2005

APPROVED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

Fig. 2-34  
REF: P-374



NOTES

1. BASE OF MAILBOX 38" - 42" ABOVE STREET FINISHED LEVEL.
2. ROOF STRUCTURE OPTIONAL



CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

MAILBOX LOCATION

REVISED BY: JAL  
DATE: 3/28/2005

APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

Fig. 2-35  
REF: KING COUNTY  
M2012

NOTES:

(1) POST SPECIFICATIONS:

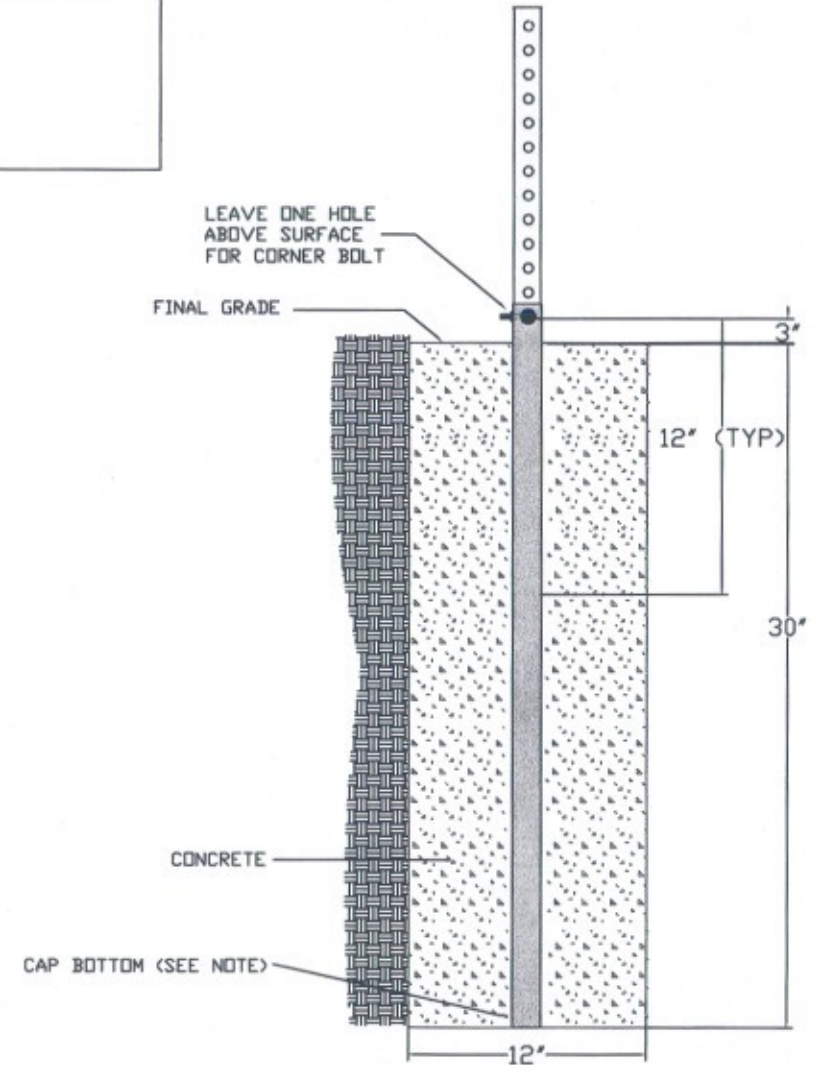
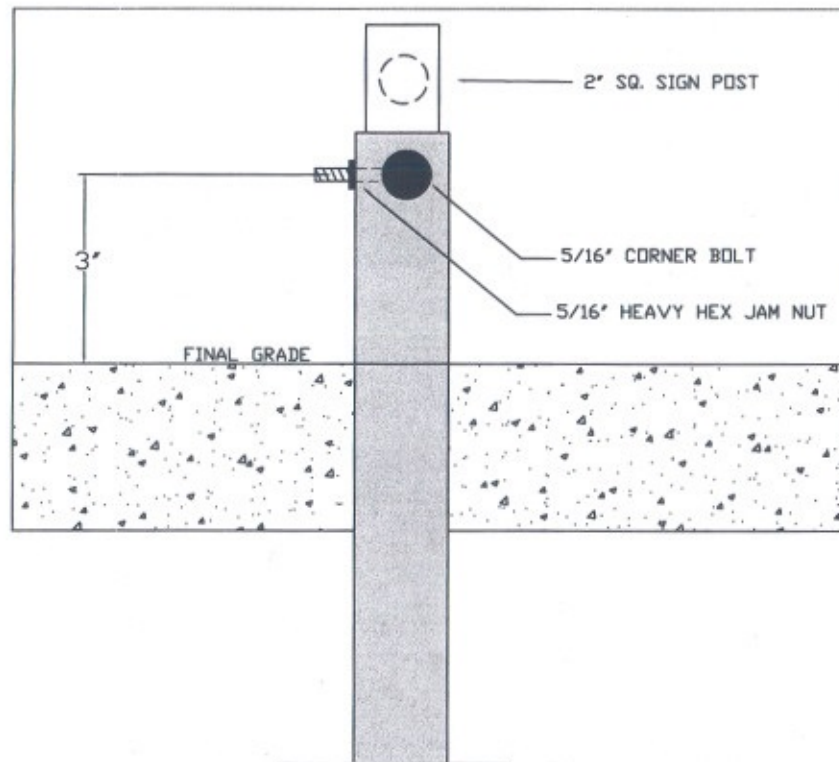
- A. 2" SQUARE POST WITH 7/16" QWICK-PUNCHED HOLES (QWIK-PUNCH TYPE OR EQUIVALENT), 1" CENTER, ON ALL FOUR SIDES FOR STANDARD SIGN SIZES.
- B. LARGER POST (2-3/16" OR 2-1/2") SIZES MAY BE REQUIRED IN COMPARISON TO SIGN SIZE.
- C. HOT DIPPED GALVANIZED 14 GAUGE STEEL WITH A CLEAR POLYMER COATING.
- D. CONFORM TO THE STANDARD SPECIFICATIONS FOR STRUCTURAL QUALITY ASTM DESIGNATION A570, GRADE 50.

(2) POST ANCHOR:

- A. GALVANIZED 7 GAUGE HEAVY DUTY 30" ANCHOR. HOLE MUST BE A MINIMUM OF 12" DIAMETER, ANCHOR MUST BE PLUMB AND LEVEL, SET AT 3" ABOVE FINAL GRADE. HUBS INSTALLED INTO SIDEWALKS NEED ONLY HALF THE AMOUNT OF CONCRETE SET INTO THE HOLE. BOTTOM OF ANCHOR MUST BE CAPPED OR TAPED TO PREVENT CONCRETE AND DIRT ENTERING FROM BOTTOM.
- B. MINIMUM POST DEPTH INSIDE OF ANCHOR MUST BE 12".

(3) SIGN HARDWARE:

- A. 3/8" STEEL DRIVE RIVET W/ 3/8" X 1" NYLON WASHER.
- B. 0.030" X 3/4" STAINLESS STEEL STRAPPING.
- C. 3/4" BAND-IT BUCKLES.
- D. GALVANIZED 5/16" CORNER BOLT WITH 5/16" HEAVY HEX JAM NUT.
- E. SIGN MOUNTING BRACKET: 2" SQ. 90 DEGREE CROSS PIECE (USE LONG SLOT 5-1/4" W/ ALLEN HEAD SET SCREWS)



CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

SIGN POST INSTALATION  
ANCHOR DETAIL  
(NOT TO SCALE)

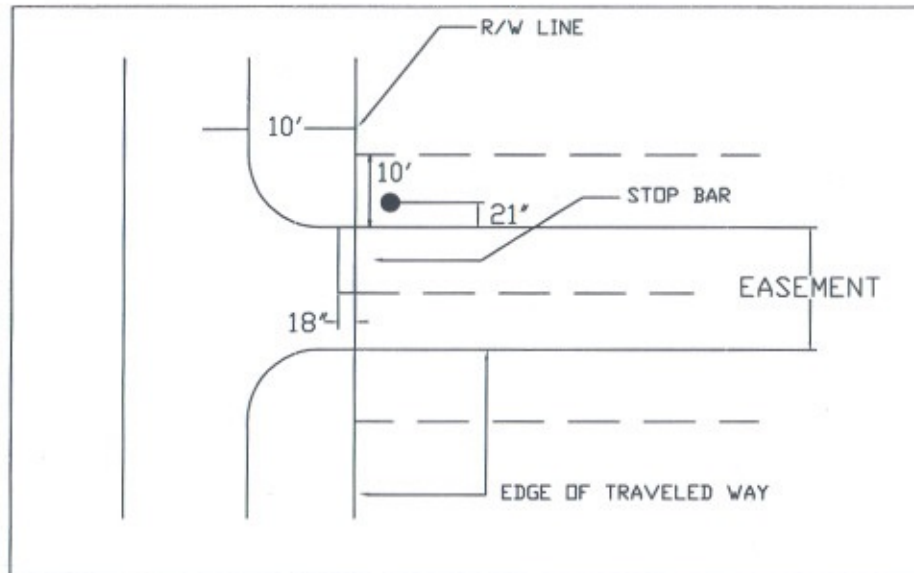
Fig. 2-36

Approval Date:

June 1, 2009

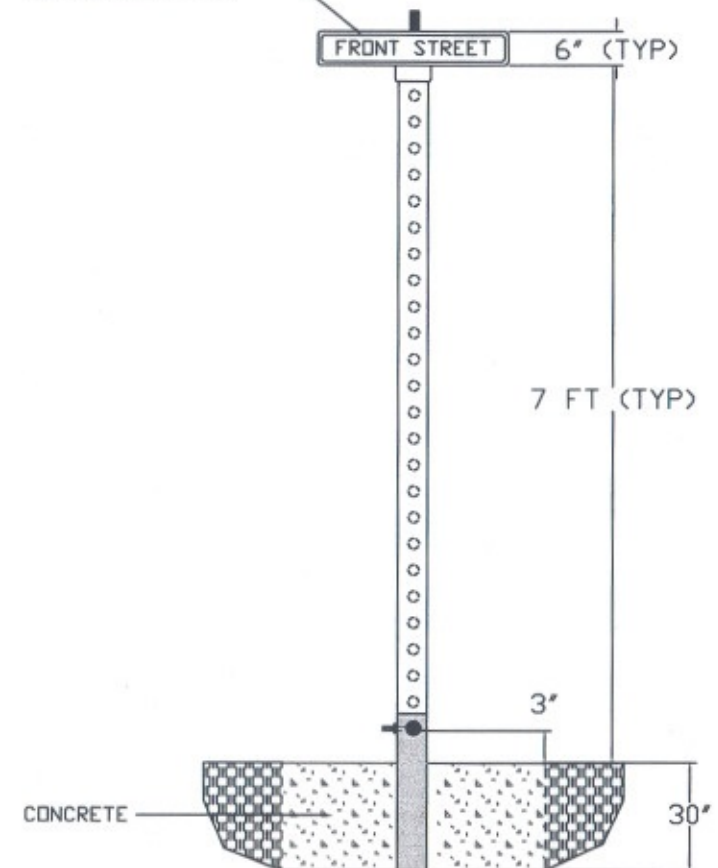
NOTES:

- A. BOTTOM OF SIGNS ARE 7 FT. FROM GRADE.
- B. STOP SIGNS SHALL BE HIGH INTENSITY GRADE SHEETING AND 30" MIN AT SPEEDS LESS THAN 25 M.P.H..
- C. STREET NAME SIGN STANDARD HEIGHT IS 6 INCHES.
- D. HOT DIPPED GALVANIZED 14 GAUGE STEEL WITH A CLEAR POLYMER COATING.
- E. STREET SIGNS WILL BE WHITE LETTERING ON GREEN BACKGROUND.
- F. SIGNS ARE TO BE "FLAT BLADE" TYPE WITH WHITE BORDER.
- G. LETTER/NUMBER STYLE WILL BE SERIES C, 4 INCHES HEIGHT/HI-WAY GOTHIC STYLE FONT.
- H. ENGINEER GRADE, REFLECTORIZED SHEETING.
- I. OTHER HEIGHTS/LENGTHS MAYBE REQUIRED.



SIGN PLACEMENT

SERIES C, 4"  
HEIGHT/HI-WAY  
GOTHIC SYLE FONT



CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

SIGN PLACEMENT  
PRIVATE / PUBLIC RD.  
(NOT TO SCALE)

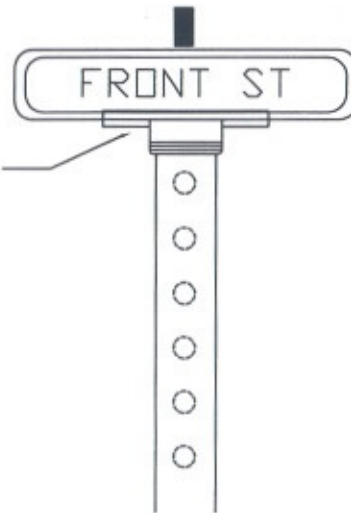
Fig. 2-37

Approval Date:

June 1, 2009



SIGN MOUNTING BRACKET:  
2" SQ. 90 DEGREE CROSS  
PIECE (LONG SLAT 5-1/4"  
ALLEN HEAD SET SCREWS)



ABBREVIATIONS:

STREET = ST  
AVENUE = AVE  
PLACE = PL  
WAY = WAY  
BOULEVARD = BLVD  
PARKWAY = PKWY  
LANE = LN  
COURT = CT  
DRIVE = DR  
ROAD = RD  
KEY = KEY  
CONNECTOR = CONN

STREET NAME SIGN DETAIL



NOTES:

- A. STREET NAME SIGN STANDARD HEIGHT IS 6 INCHES.
- B. LETTER/NUMBER STYLE WILL BE SERIES C, 4 INCHES HEIGHT/HI-WAY GOTHIC STYLE FONT.
- C. SIGNS ARE TO BE "FLAT BLADE" TYPE WITH WHITE BORDER.
- D. ENGINEER GRADE, REFLECTORIZED SHEETING.
- E. OTHER HEIGHTS/LENGTHS MAY BE REQUIRED.
- F. SIGNS 36" OR LONGER REQUIRE 12" SLOT.



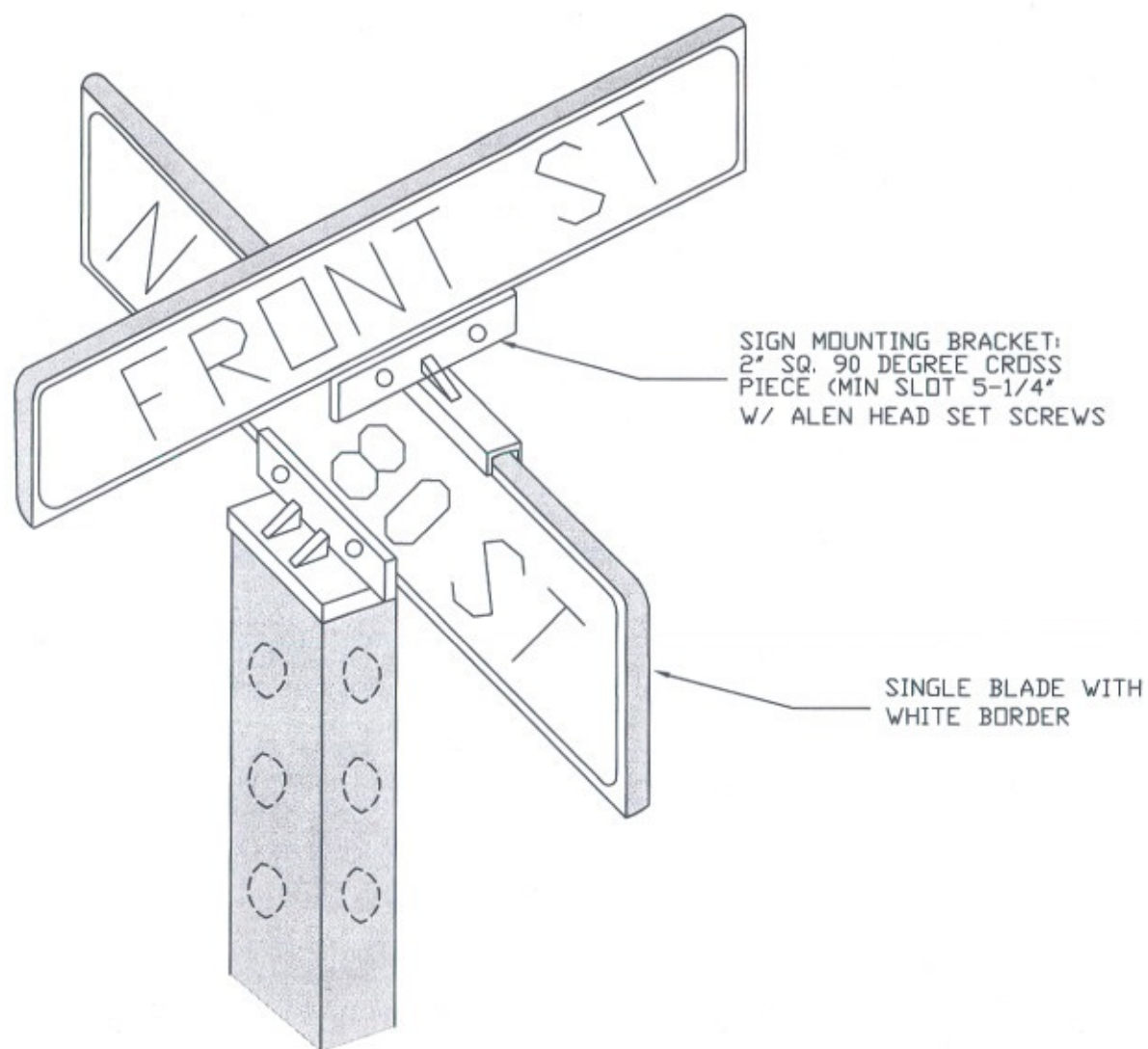
CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

SIGN FABRICATION DETAIL  
(NOT TO SCALE)

Fig. 2-38

Approval Date:

June 1, 2009



SEE DWG. 1-01 FOR DETAIL OF TYPICAL POST INSTALLATION



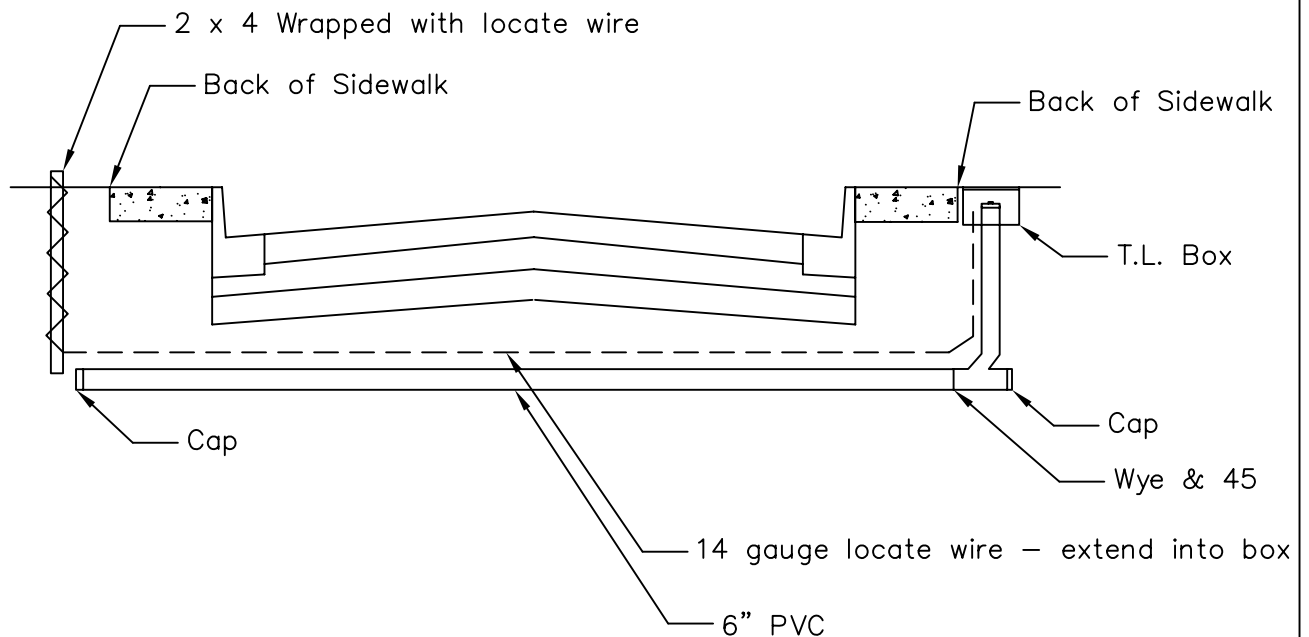
CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

SIGN STANDARDS  
(NOT TO SCALE)

**Fig. 2-39**

Approval Date:  
**June 1, 2009**





1. The Conduit location(s) will be staked in the field by the Engineer. The conduit material shall be 6" PVC. The West side shall be brought out to finished grade and placed in a Fogtite B-9 T.L. box or approved equal. The East side shall be marked with a treated 2x4 wrapped with the locate wire. The board shall be cut off 6" above finished grade.
2. Minimum conduit cover (burial depth) shall be 3 feet. Actual cover depth will vary due to existing utilities, and shall be as determined by the City Engineer or City Inspector.



CITY OF POULSBO  
DEPARTMENT OF PUBLIC WORKS

TYPICAL ROADWAY CONDUIT  
CROSSING DETAIL

REVISED BY: MJB  
DATE: 5/12/09

APPROVED BY: \_\_\_\_\_  
DATE: June 1, 2009

Fig. 2-50